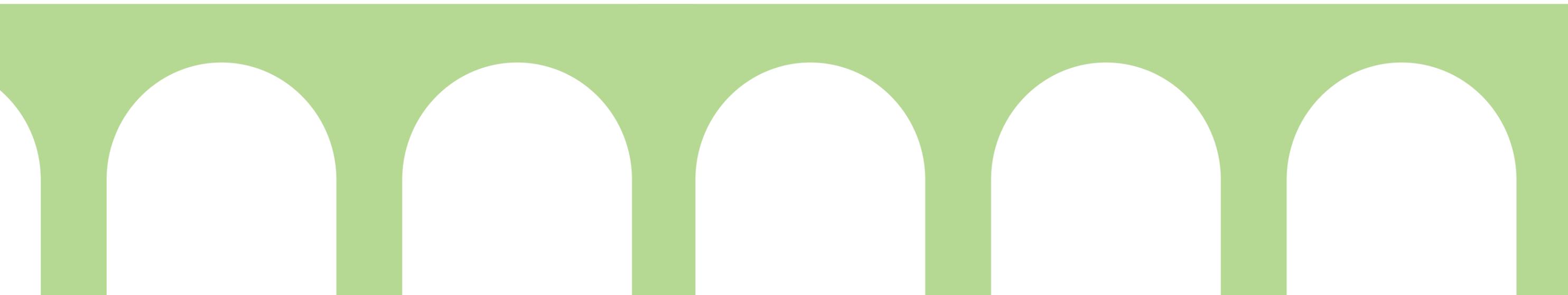


PART D

Character areas



4.1 Character Areas

4.1.1 Overview

- » This section of the SPD provides more detail on the various character areas that will be created through its delivery.
- » Six distinct, discrete but connected character areas emerge. Buildings and landscape play a critical role in establishing character. But it is people and their activities – living, playing, working, making, learning – which will bring life to these areas. Six character areas have been identified, as follows:
 - Blackstock Street
 - Kingsway Park
 - Canal Square & City College
 - City fringe and gateway
 - Railway edge
 - Oriol Street & Cherry Lane
- » For each character area, plans, views, artist’s impressions, and precedent images are provided to illustrate the opportunities available. Grounded in the site-wide masterplan framework outlined in the previous chapter, they establish the key design principles which development proposals should align with.
- » Each character area has been explored and illustrated through the key characteristics of the National Model Design Code: Vision & Identity, Movement, Public Space and Nature, Built Form and Uses.

Spatial guidance: Character Areas

- a. Applicants must demonstrate a good understanding of the character area where their site is situated in the Design and Access Statement.
- b. Development proposals must enhance the character and identity of the character area in which they are located and should reflect the design guidance set out in this SPD.

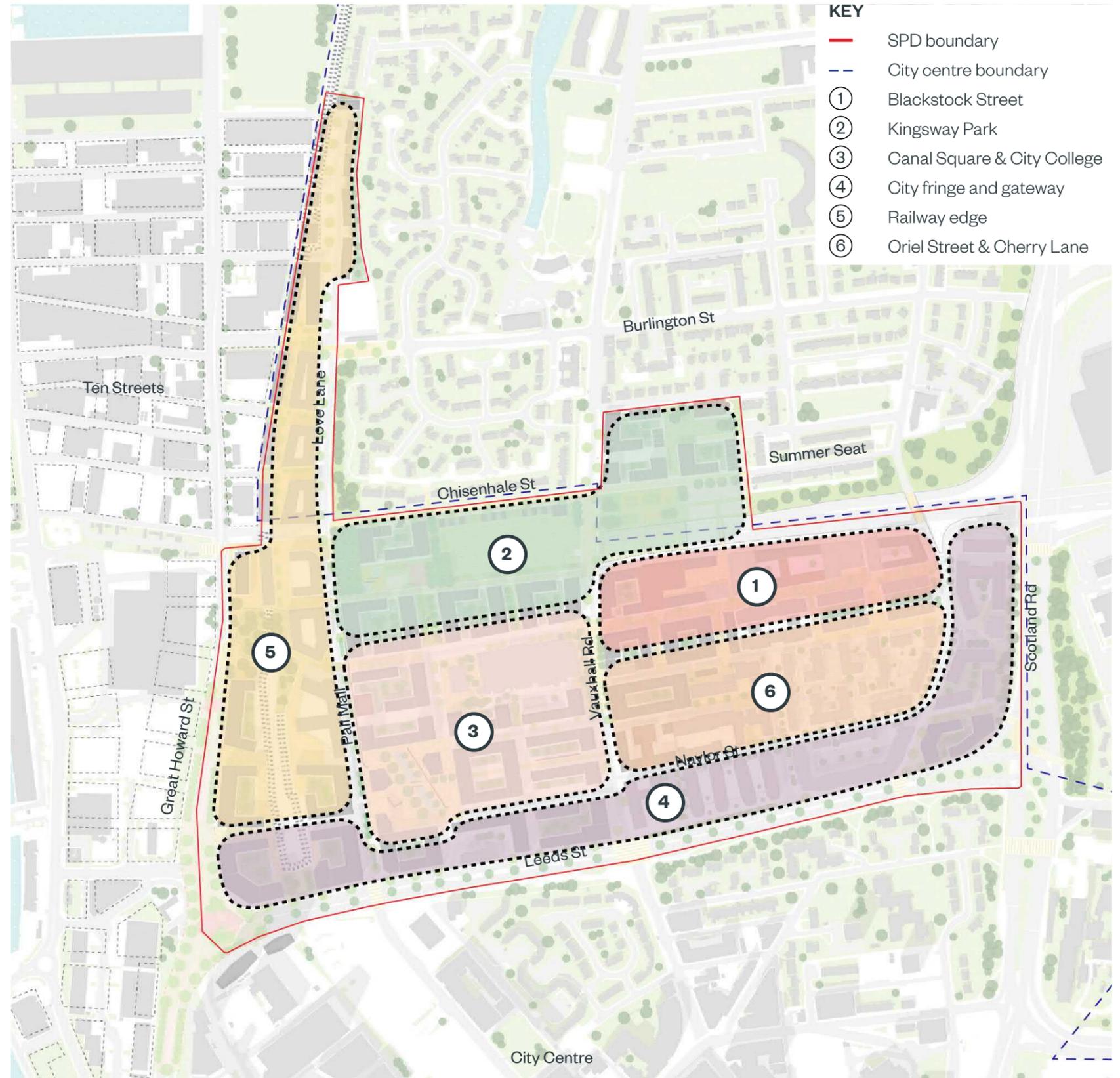


Fig 77 Character Areas

4.2 Blackstock Street

4.2.1 Vision & identity

- » Blackstock Street will become a vibrant, employment-led mixed-use area, retaining its distinctive warehouse character through a combination of conversions and new infill development.
- » Existing employment uses will be integrated with new businesses, occupying restored warehouses and sheds, alongside new buildings with non-residential ground floors and residential uses above.
- » The public realm will reflect the area's activity and energy, enabling businesses to spill out and activate the street.
- » A new pedestrian, wheeling and cycling bridge connection will enhance access between Blackstock Street, and the new Kingsway Park and residential neighbourhoods north of the Kingsway Tunnel.

Spatial guidance: Vision & identity

- a. Development proposals within the Blackstock Street Character Area must demonstrate how they contribute to the vision for the area, as set out in this SPD.

KEY			
	SPD boundary		Landmark building
	Heritage assets		SuDS
	SPD proposed buildings		Blackstock Yard
	Non-residential frontage		Limekiln Lane Bridge (existing)
	Inset balconies		Kingsway Bridge (proposed)



Fig 78 Blackstock Street Character Area

4.2.2 Movement

Blackstock Street

- » Blackstock Street will be transformed into a pedestrian priority route with tree planting and Sustainable Drainage Systems (SuDS) running along its entire length.
- » On-street parallel parking and loading/servicing bays will be integrated alongside the trees to ensure parked cars do not dominate the streetscape.
- » Vehicular gateways at either end will allow the street to function as a pedestrian-priority route at certain times of the day, supporting spill-out activities from non-residential ground-floor uses.

Ford Street

- » Ford Street will retain its role as a servicing road for the employment uses fronting the Kingsway Tunnel.
- » Two-metre-wide footways will be provided on either side of the road, ensuring safe east-west pedestrian permeability.

Spatial guidance: Movement

- a. Development proposals within the Blackstock Street Character Area must demonstrate how they respond to and contribute to the design principles set out in this SPD for Blackstock Street and Ford Street.

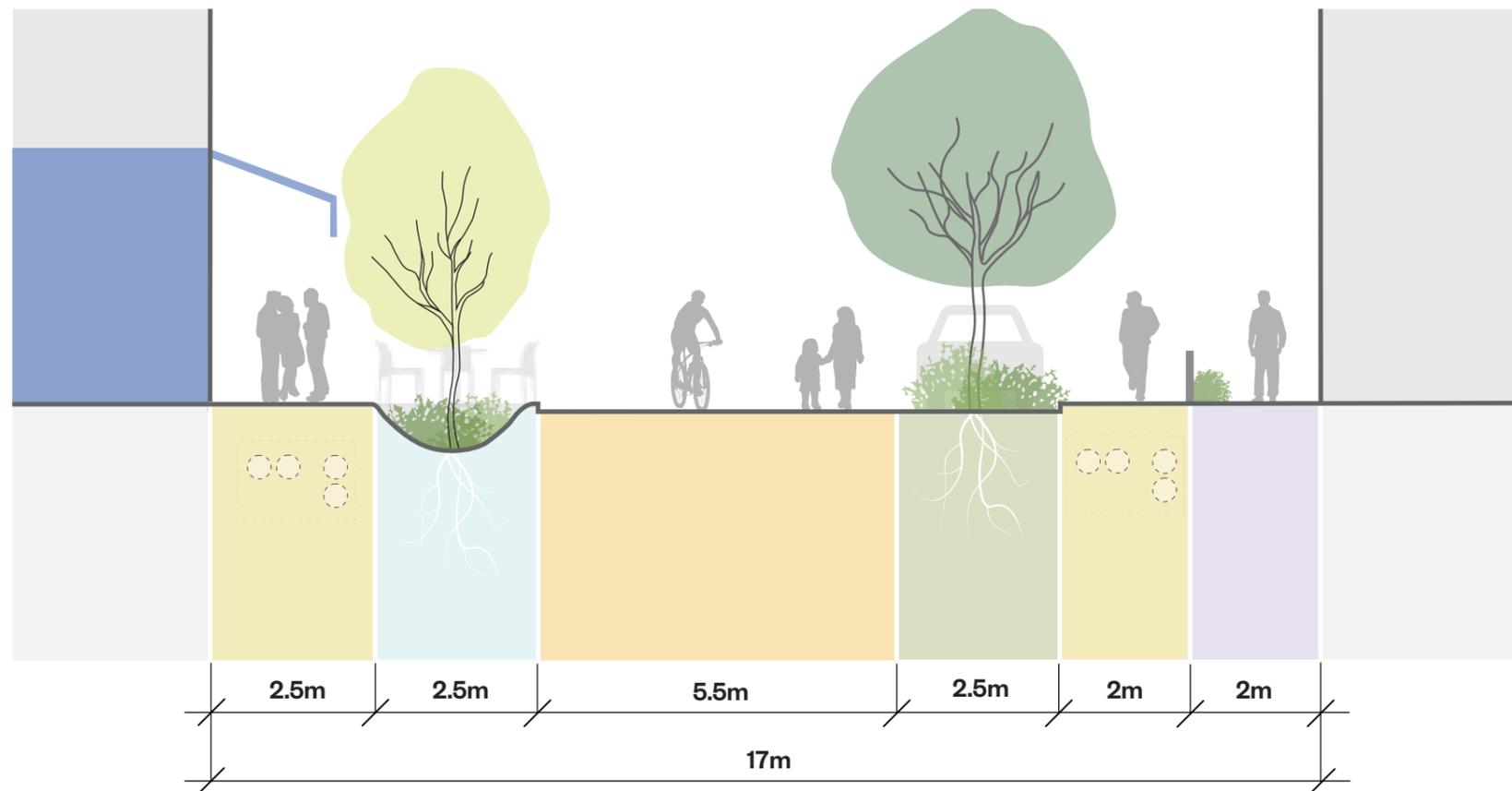


Fig 80 Blackstock Street proposed section



Fig 79 Blackstock Street today

KEY

- Carriageway
- Footpath with underground service corridor
- Tree/planting zone with on street parking
- Building zone
- Planted swale

4.2.3 Public space and nature

Blackstock Yard

- » Blackstock Yard will act as a catalyst for regeneration in the area, enclosed by revitalised warehouses and sheds reimagined for employment uses, with infill development delivering employment spaces along Ford Street and mixed-use buildings—employment at ground floor and residential above—along Blackstock Street.
- » The yard will function as a well-overlooked, semi-public space, with the potential to be secured at night after business hours.
- » In the longer term, it will become a key stepping stone towards Kingsway Park, connected via a new Kingsway Bridge over the Kingsway Tunnel.

Spatial guidance: Blackstock Yard

- The existing warehouses and sheds should be retained, forming a defining framework for the yard and contributing to its identity and character.
- Non-residential uses surrounding the yard must ensure a continuous and vibrant active frontage at ground floor level, with the potential to spill out into the courtyard.
- The yard should incorporate tree planting, lighting, high-quality seating and cycle parking, fully integrated into the public realm design.
- Development proposals around Blackstock Yard must not prejudice the long-term vision for a pedestrian link connecting Blackstock Street, Ford Street, and the future Kingsway Bridge.

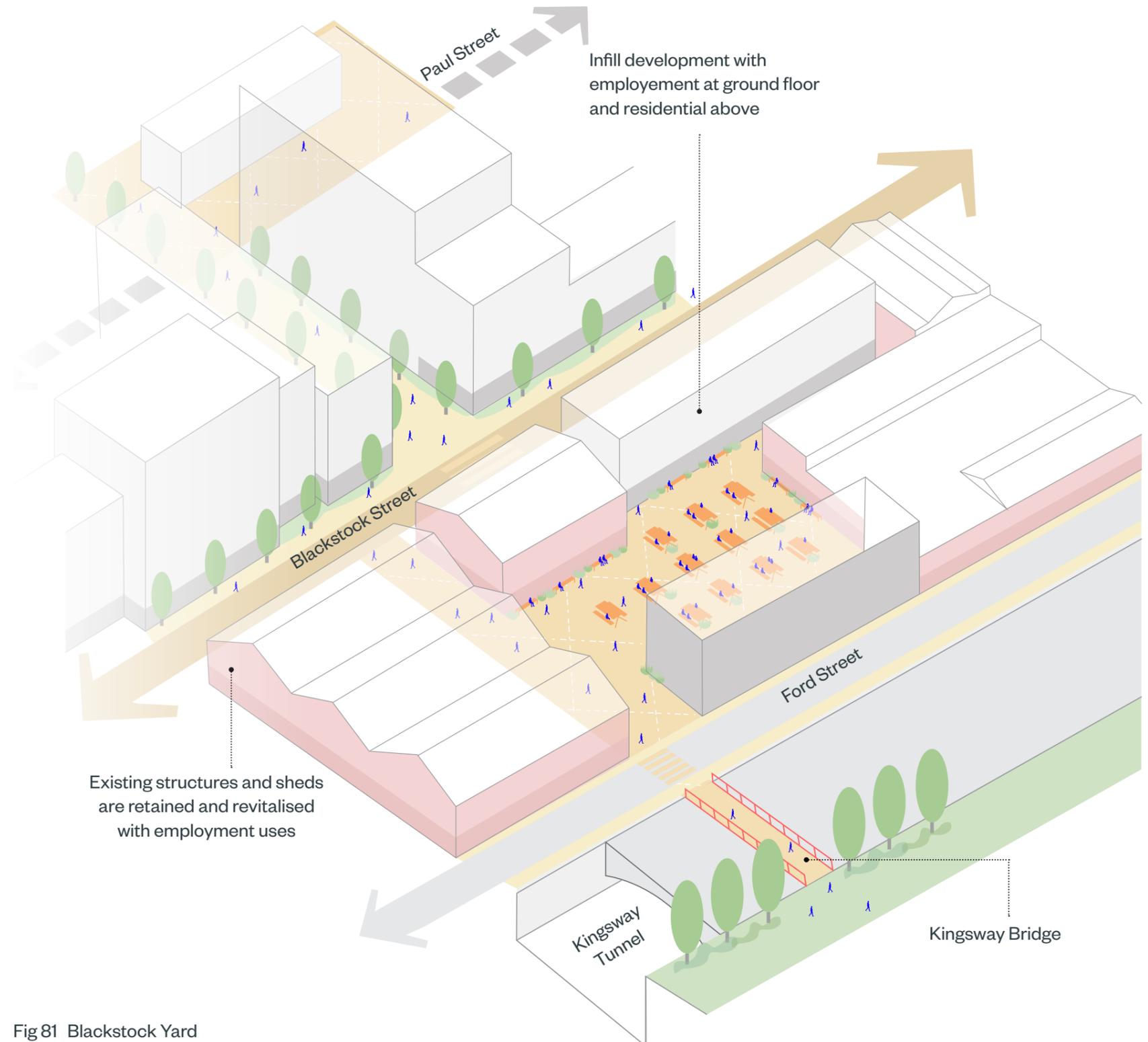


Fig 81 Blackstock Yard

4.2.4 Built form

- » Several buildings within this character area have been identified for retention. The retention and re-use of non-designated assets – including the brick chimney and a number of warehouse buildings – will help to deliver sustainable development and contribute positively to placemaking and urban character.
- » Brick warehouses have the potential to be re-used by retaining their original brick envelopes, enhanced with interventions such as new openings and roof or side extensions.
- » Existing sheds offer the opportunity to be re-used by stripping back to their original steel frame structures and recladding them with contemporary metal materials.
- » A mix of building heights, ranging from 3-6 storeys, will create variety to the skyline and maximise daylight and sunlight into the courtyards and public realm.
- » The least constrained and sensitive area is considered to be the eastern end of Blackstock Street. This part of the site is appropriate for a marker building.

Spatial guidance: Built form

- a. The built form of infill development should respond positively to the setting of non-designated heritage assets.
- b. Both residential and non-residential frontages should promote passive surveillance and contribute to a safe and welcoming public realm.
- c. For ground floor non-residential uses, internal spaces that generate activity - such as reception areas or meeting rooms - should be located along the frontage to animate the street.
- d. Building lines should be brought to the site edge to activate the street, avoiding the use of boundary fencing and maintaining a human scale.
- e. Development should feature a varied roofscape and use materials that reference the area's industrial heritage - such as pitched or sawtooth roofs and red brick façades.
- f. Roof forms should be designed to maximise photovoltaic (PV) coverage and bring natural daylight into internal spaces.
- g. Rooftop plant, lift overruns, and building maintenance units (BMUs) must be set back from main façades and integrated into the overall roofscape to remain hidden from street-level views.
- h. Balconies along Blackstock Street must be inset.
- i. Single-aspect residential homes facing the Kingsway Tunnel must be avoided.

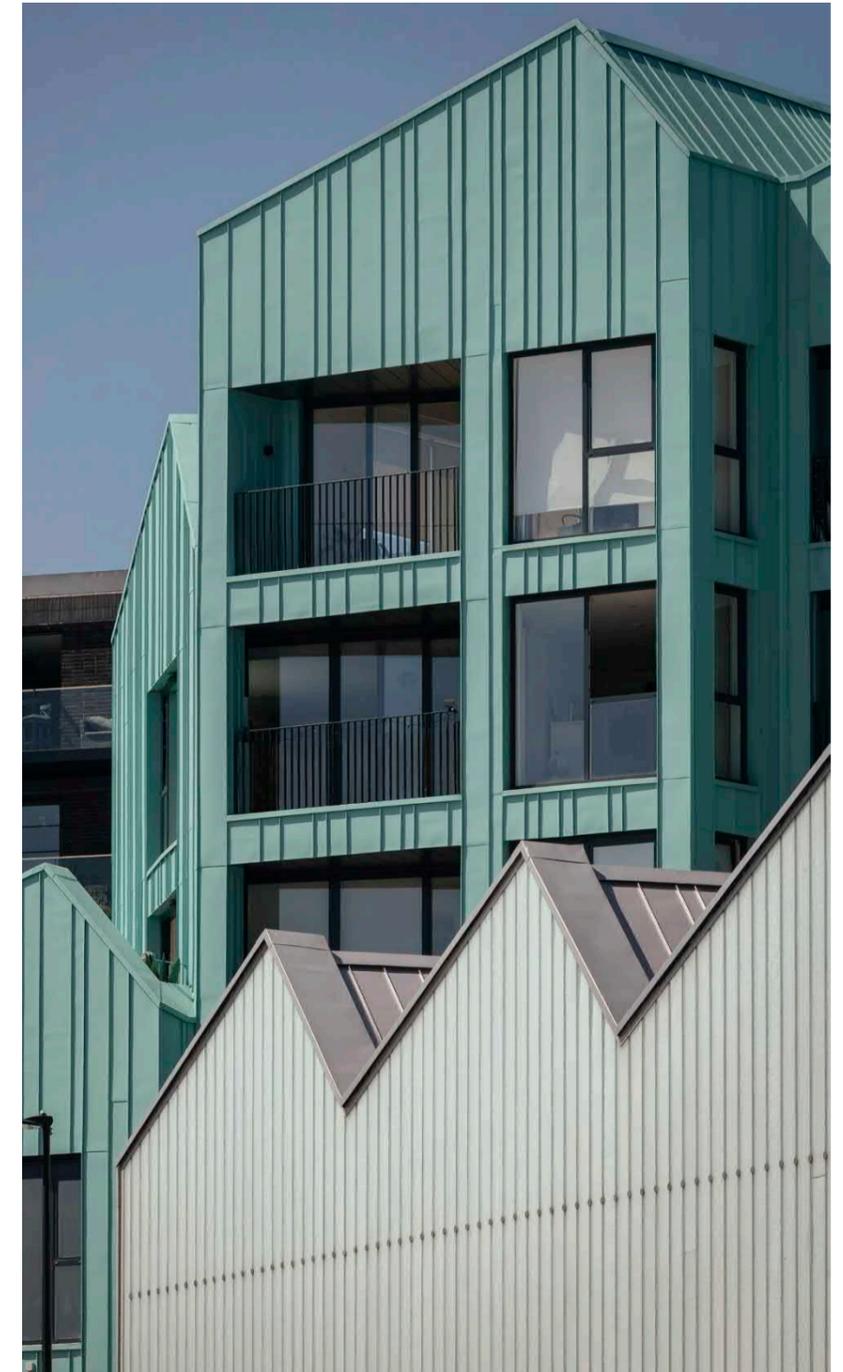


Fig 82 The mixed-use development draws inspiration from the area's industrial heritage, featuring elements such as a saw-tooth roof and metal cladding.



Blackstock Street will be transformed into a dynamic, employment-led neighbourhood that celebrates its warehouse heritage through adaptive reuse and thoughtful new development, blending vibrant workspaces with homes and a lively public realm that energises the street.

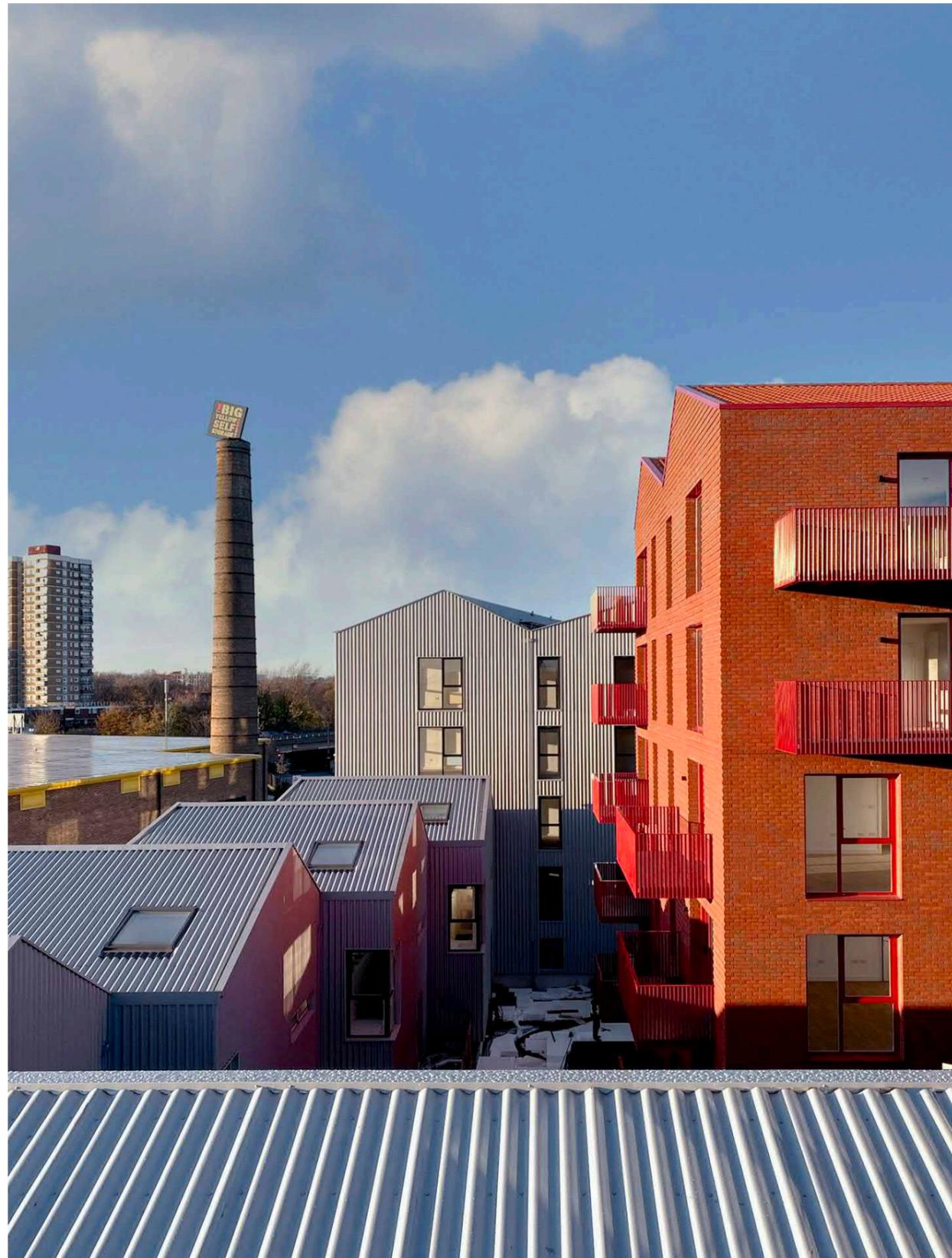


Fig 83 New development incorporating light industry and new dwellings.

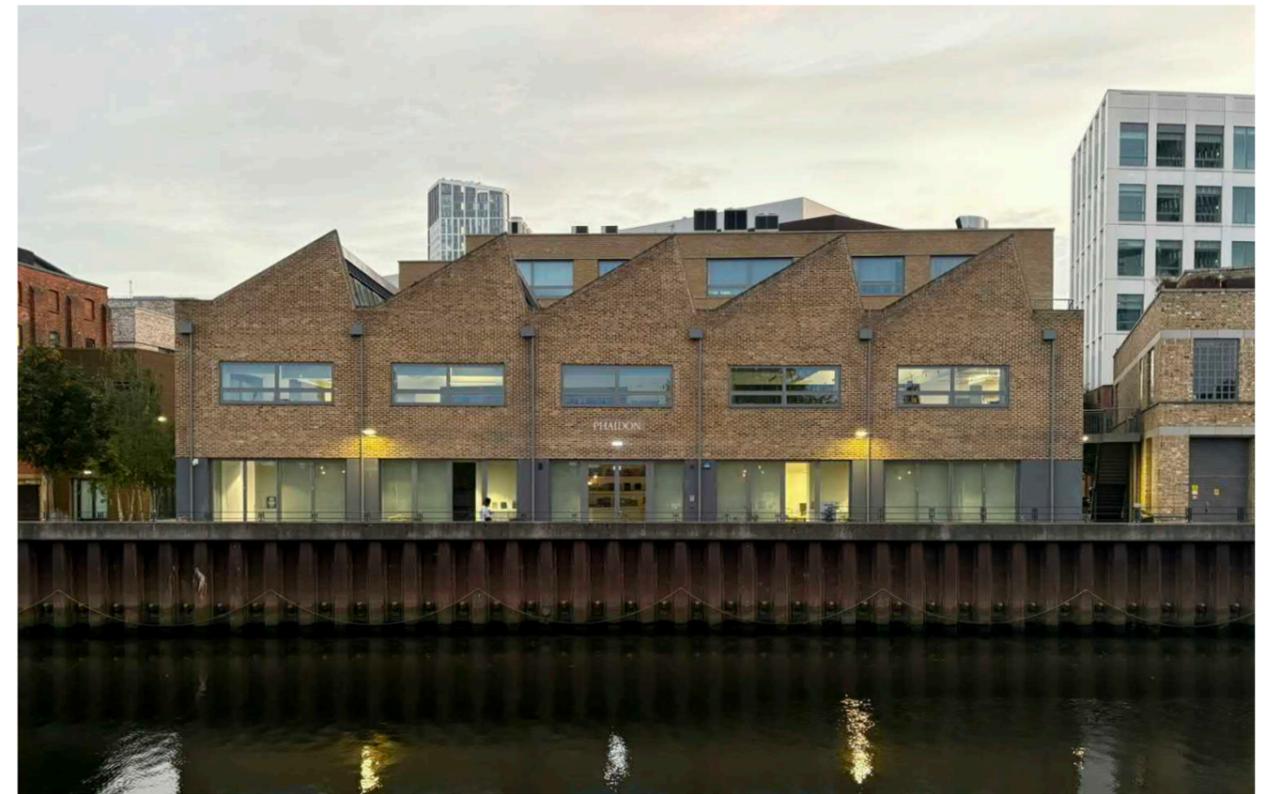


Fig 84 Saw-tooth roof to new brick building accommodating employment, responding to the area's rich industrial past.



Fig 85 Yard space with employment and commercial uses.

4.2.5 Uses

- » This area will retain its character as an employment-led zone.
- » Existing and new infill employment uses will line Ford Street, creating a buffer against noise and pollution from the Kingsway Tunnel and protecting the residential areas to the south.
- » Blackstock Street will feature employment uses at ground floor level along both its northern and southern edges, with residential uses above.
- » These employment spaces will include workshops, light industry, and studios for creatives and makers.

Spatial guidance: Uses

- Development proposals should incorporate employment uses at ground floor level along Blackstock Street.
- Along Ford Street, proposals should provide employment uses only, with residential accommodation permitted on upper floors at the junction of Ford Street and Vauxhall Road.



Fig 86 Flexible workshop spaces to support a range of uses by makers and creatives.

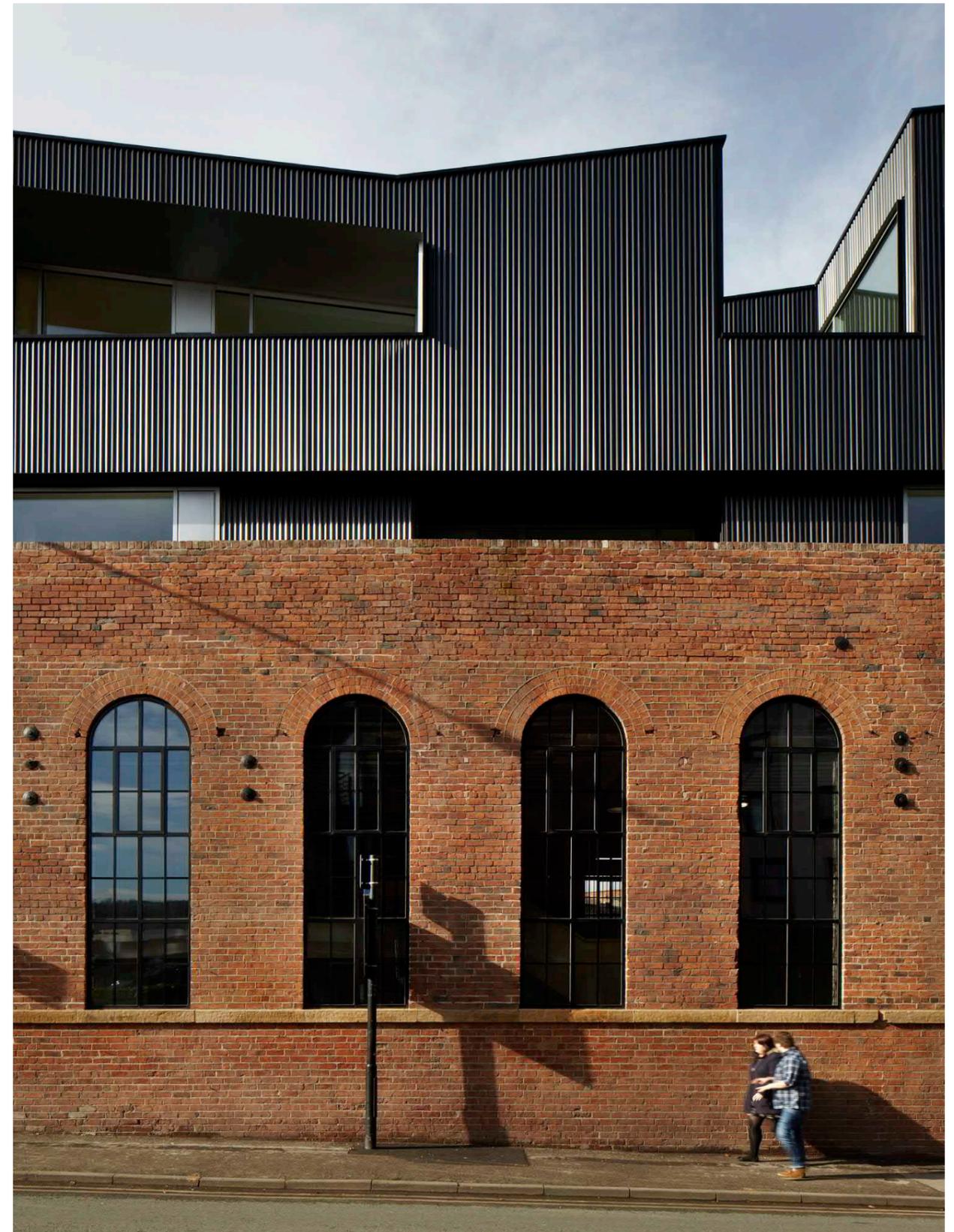


Fig 87 Mixed-use building with upward extension to a Victorian industrial brick building.

4.3 Kingsway Park

4.3.1 Vision & identity

- » Kingsway Park will become a vibrant, residential-led area, developing along a new linear park and biodiversity corridor.
- » The park will provide spaces for play, sport, rest, and relaxation for both new and existing residents, while also improving east-west pedestrian and cycling movement linking the emerging Central Park (Liverpool Waters) with Ennerdale Park and Everton Park beyond.
- » The area will offer a sensitive transition between the larger buildings south of the Kingsway Tunnel and the low-rise residential neighbourhood to the north, bringing them together into a cohesive whole.
- » A new pedestrian/cycling bridge will link Kingsway Park and Blackstock Street, improving north-south pedestrian/cycling movement.

Spatial guidance : Vision & identity

- a. Development proposals within the Kingsway Park Character Area must demonstrate how they contribute to the vision for the area set out in this SPD.

KEY

	SPD boundary		Kingsway Park
	Heritage assets		Memorial Gardens
	SPD proposed buildings		Chisenhale Bridge & The Bridge PH
	Non-residential frontage		Kingsway Bridge (proposed)
	Inset balconies		Church of Our Lady of Reconciliation de la Salette
	Landmark building		Church Green
	SuDS		

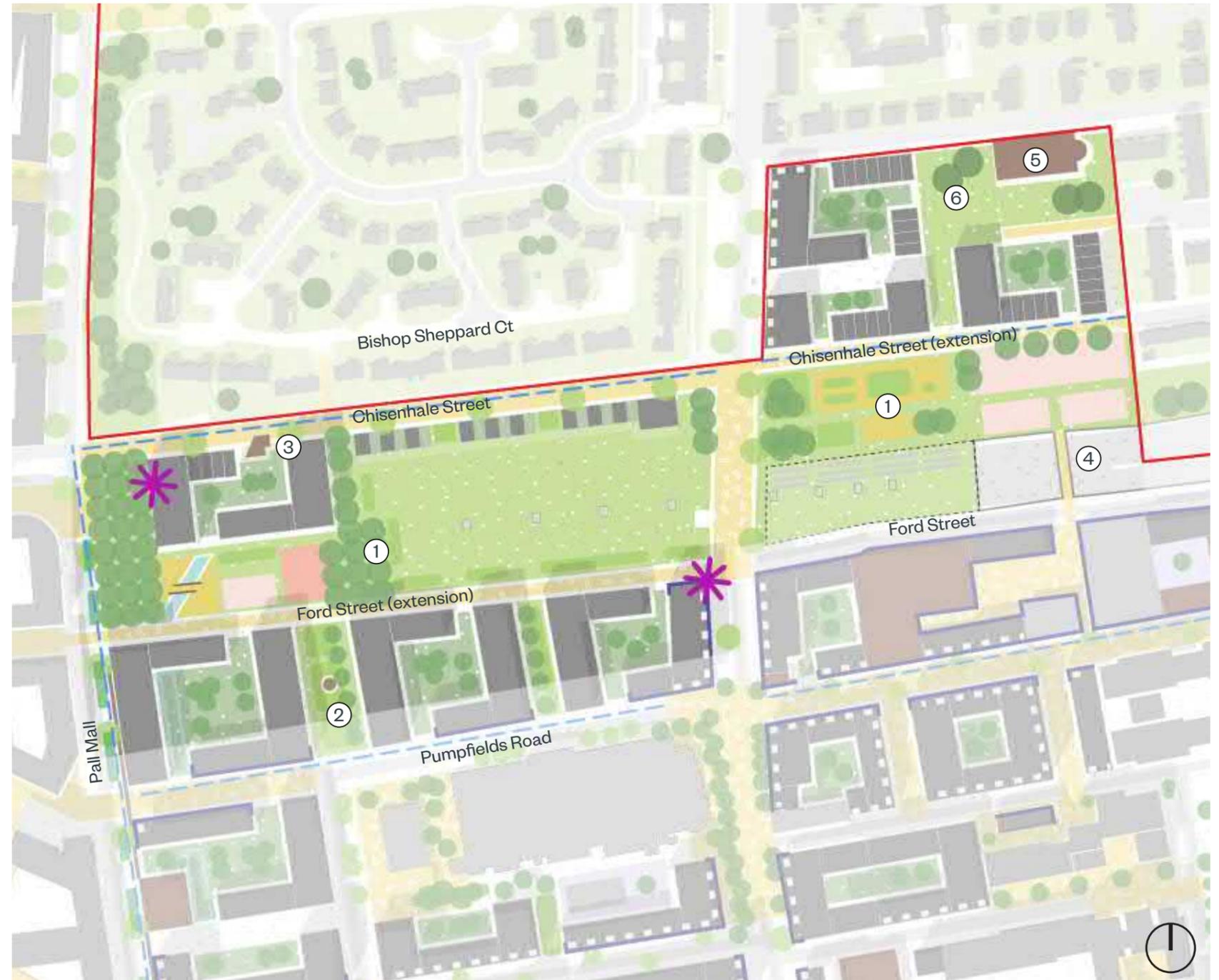


Fig 88 Kingsway Park Character Area

4.3.2 Movement

Chisenhale Street

- » Chisenhale Street will be transformed into a traffic calmed route with tree planting and Sustainable Drainage Systems (SuDS) running along its entire length.
- » On-street parallel parking will be integrated alongside the trees to ensure parked cars do not dominate the streetscape.
- » Traffic calming measures via surface treatments will prevent rat-running and prioritise pedestrian, wheeling and cycling movement.

Chisenhale Street (extension)

- » Chisenhale Street will be extended eastward via an emergency/servicing only route linking to Summer Seat and allowing east-west pedestrian, wheeling and cycling permeability.

Ford Street (extension)

- » Ford Street will be extended westward via an emergency/servicing only route linking to Pall Mall and allowing east-west pedestrian, wheeling and cycling permeability.

Spatial guidance: Movement

- a. Development proposals within the Kingsway Park Character Area must demonstrate how they respond to and contribute to the design principles set out in this SPD for Chisenhale Street and its extension, and Ford Street extension.

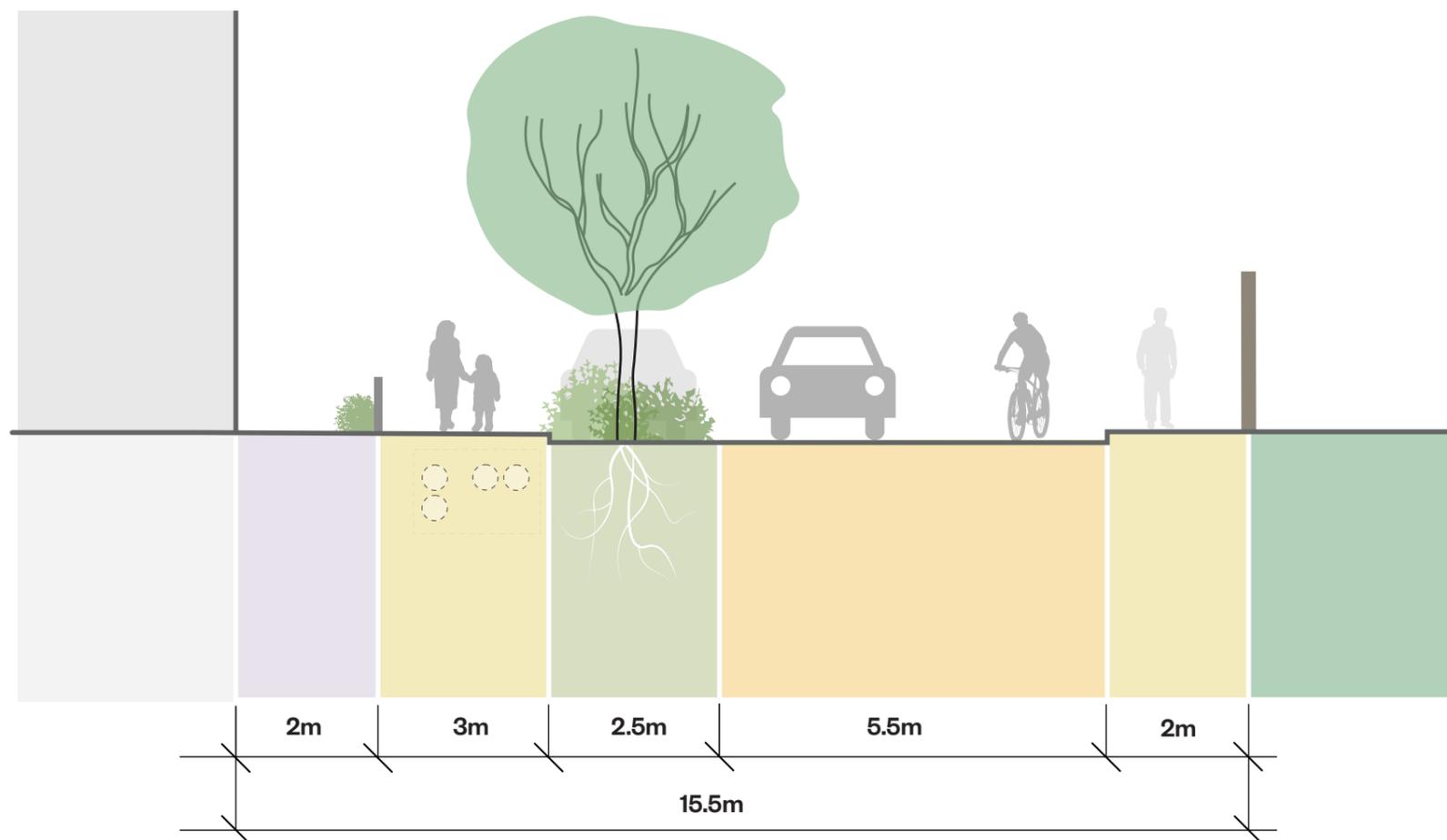


Fig 90 Chisenhale Street proposed section



Fig 89 Chisenhale Street today

KEY

- Carriageway (traffic calmed)
- Footpath with underground service corridor
- Tree/planting zone with on street parking on carriage way side
- Existing rear gardens (Eldonian Village)
- Building zone

4.3.3 Public space and nature

Kingsway Park

- » Aligning with the Liverpool Waterfront SPD, Kingsway Park is a new east–west linear park and biodiversity corridor of approximately 2.4 hectares, located above the Kingsway Tunnel and forming the green backbone of the SPD masterplan.
- » The park occupies an area historically lacking in open green space - where rural fields were rapidly replaced with factories during the Industrial Revolution.
- » Set within a broader network of existing and emerging green spaces, Kingsway Park links the planned Central Park of Liverpool Waters to the established landscape of Ennerdale Park. In doing so, it improves east-west pedestrian/cycling/ wheeling movement while also supporting north–south movement, connecting residential neighbourhoods north of the tunnel to new communities in the south, with green spurs extending into the city centre.
- » Kingsway Park will function as a multifunctional open space - offering inclusive play for all genders, abilities and ages, sports facilities such as pitches, padel courts, a basketball court, and MUGAs, alongside areas for rest, relaxation, and communal growing.
- » The design responds to the constraint of the tunnel beneath by locating tree planting where soil depth allows, primarily along the park's western edge.

Spatial guidance: Kingsway Park

- a. There must be shared pedestrian, wheeling and cycling routes through the park, connecting between Chisenhale Street (and its eastward extension to Summer Seat) and the westward extension of Ford Street.
- b. The park must include recreational spaces for communal growing, play and picnics as illustrated in the open space and play strategy.
- c. The park must allow for Neighbourhood Equipped Area for Play (NEAP) for all ages and Adventure Playground.
- d. Sport provision must be included for 12+ and Youth play including a basketball court, sport pitches and MUGA.





Kingsway Park will evolve into a green and connected neighbourhood that unites new and existing communities through a biodiverse linear park, seamlessly blending new homes, active travel, play and inclusive public spaces into a cohesive and thriving urban landscape.



Fig 91 Footpath defined by planting edges, boosting biodiversity



Fig 92 Water feature with integrated seating within a park setting

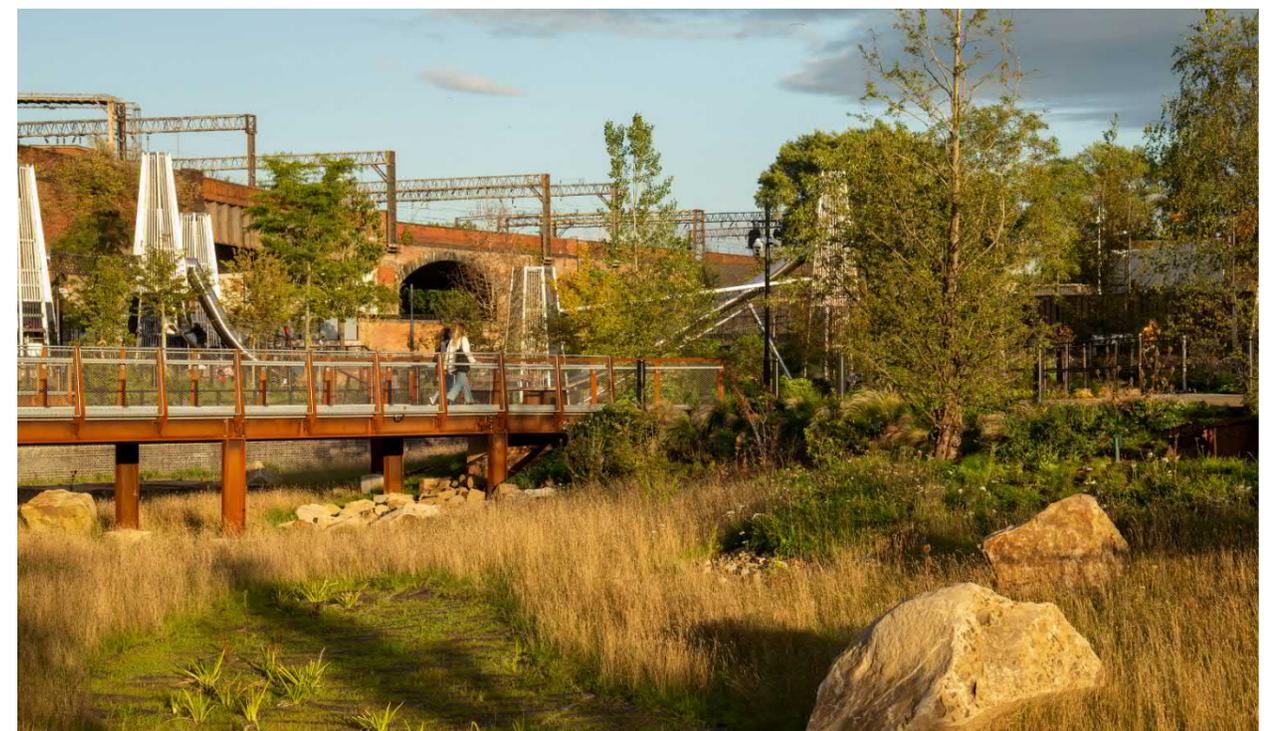


Fig 93 New park along a railway viaduct, celebrating the rich industrial heritage of the place.



Fig 94 Community garden



Fig 95 Sports court



- KEY**
- ① Water feature and bridge
 - ② Basketball court
 - ③ Raised planters
 - ④ Sports courts
 - ⑤ Adventure playground
 - ⑥ Community growing
 - ⑦ Memorial Gardens
 - ⑧ Private courtyard
 - ⑨ Blackstock Yard
 - ⑩ Eldonian Village
 - ⑪ Chisenhale Bridge and The Bridge PH
 - ⑫ Kingsway Bridge
 - ⑬ Wildflower meadow

4.3.4 Built form

- » Built form and massing within the Kingsway Park Character Area will support the transition from the larger, taller buildings proposed to the south of the Kingsway Tunnel to the low-rise residential neighbourhoods of North Liverpool.
- » Typologies within this area will include low-rise apartment buildings, maisonettes, and three-storey townhouses.
- » Lower building heights will also help create a more sensitive relationship with the surrounding heritage context, including the listed Church of Our Lady of Reconciliation de la Salette, as well as the non-designated heritage assets of the Bridge Public House and Chisenhale Bridge.
- » The focus within Kingsway Park will be on providing larger family housing - three- and four-bedroom homes - arranged around communal courtyards or using a back-to-back layout to help secure the boundaries of rear gardens.

Spatial guidance: Built form

- a. The built form should respond positively to the setting of both designated and non-designated heritage assets, i.e. the Church of Our Lady of Reconciliation de la Salette, the Bridge Public House, and Chisenhale Bridge.
- b. Apartment buildings should maximise the number of dual-aspect dwellings, avoid single-aspect north-facing units, minimise internal corridor lengths, and ensure high standards of natural light and ventilation.
- c. Buildings should be arranged around communal courtyards that provide secure amenity space for all residents, along with doorstep play.
- d. Frontages should promote passive surveillance and contribute to a safe, active, and welcoming public realm.

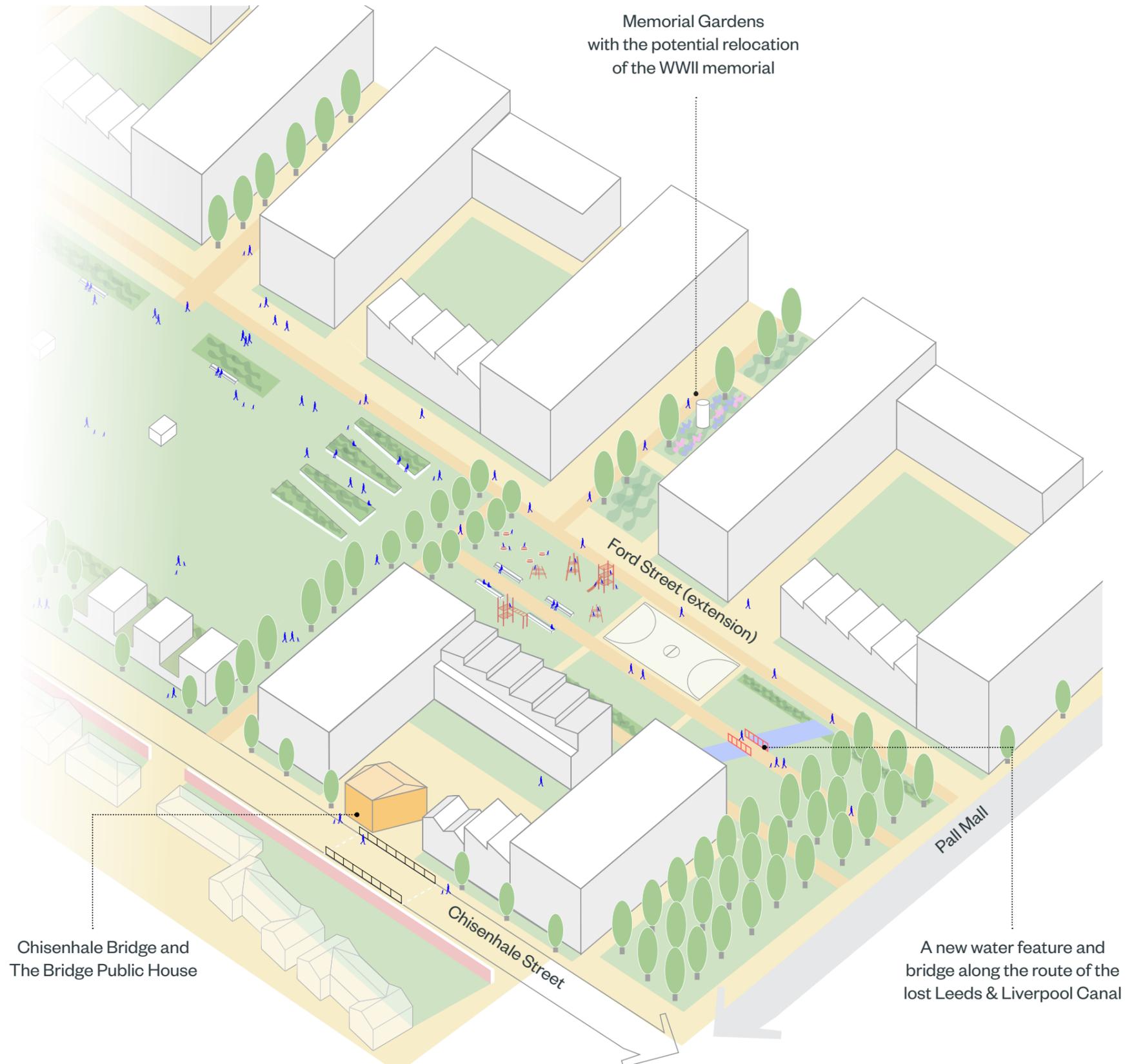


Fig 96 Kingsway Park

Spatial guidance: Built form

- e. Development proposals along Kingsway Park must engage positively with the park's boundary and should incorporate green thresholds, i.e. front gardens defined by planting and hedges while minimising the use of brick walls.
- f. Building heights within this area should range from six to three storeys, with lower heights concentrated around the listed church and a taller building along Pall Mall.
- g. Balconies facing Kingsway Park must be projecting, maximising views towards the park and nature.
- h. Roofscape forms should take cues from the area's character and industrial heritage. Pitched roofs, sawtooth roofs, and flat roofs will be considered acceptable, while mansard roofs must be avoided.



Fig 97 Four storey building along a linear park with planting defining front gardens

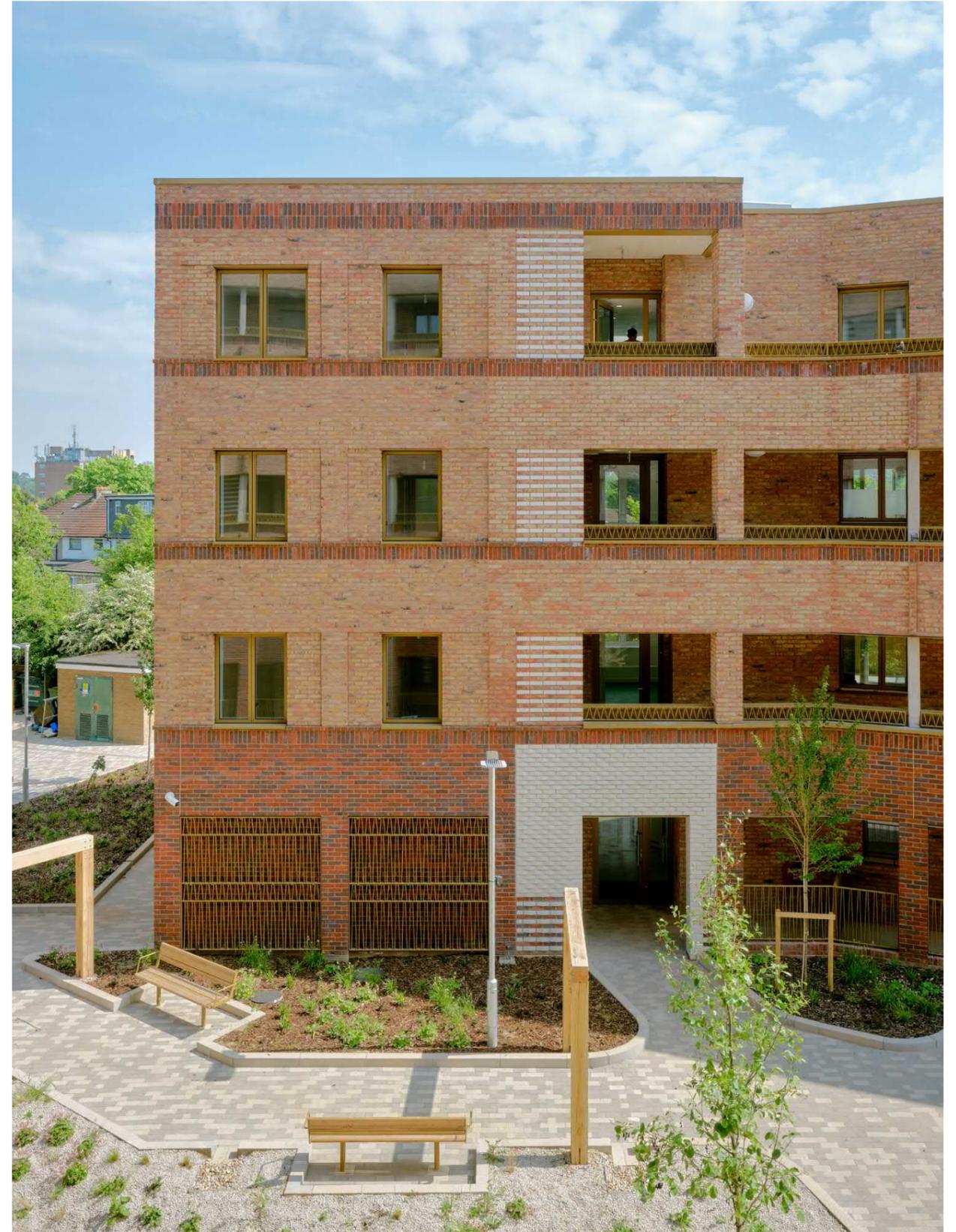


Fig 98 Low rise apartment building



Chisenhale Street will be reimagined as a green, traffic calmed route, featuring tree planting, SuDS, integrated parking, and traffic calming measures that enhance access for residents while discouraging through-traffic and promoting active travel.

4.3.5 Uses

- » Kingsway Park will be a residential-led area with potential for ground floor commercial uses along the Vauxhall Road frontage and ground floor employment uses along Pumpfields Road.
- » Along Chisenhale Street the Bridge House PH will be retained to provide commercial uses (Pub).
- » Active frontage will be ensured throughout via front doors on street (maisonettes/houses and ground floor flats).

Spatial guidance: Uses

- a. Development proposals within the Kingsway Park Character Area must demonstrate how they respond to and contribute to the design principles set out in this SPD for Chisenhale Street and its extension, and Ford Street extension.



Fig 99 Front gardens defined by planting and trees



Fig 100 Planting edge to front gardens with inset railings to maximise passive surveillance from ground floor windows

4.4 Canal Square & City College

4.4.1 Vision & identity

- » The Canal Square & City College character area lies between Pumpfields Road to the east and Pall Mall to the west.
- » This area will form the civic heart of the SPD, accommodating a mix of educational, community, cultural, commercial, and employment uses, integrated with apartments and heritage structures.
- » A truly mixed-use destination, it will serve the new SPD neighbourhoods and the residential area to the north, while also attracting visitors from the city centre.
- » Once home to bustling warehouses and canal docks, the area will once again see water flowing - celebrating and reconnecting with its rich industrial past.

Spatial guidance: Vision & identity

- a. Development proposals within the Canal Square & City College Character Area must demonstrate how they contribute to the vision for the area set out in this SPD.

KEY			
	SPD boundary		Canal Square
	Heritage assets		City College
	SPD proposed buildings		Vauxhall Square
	Non-residential frontage		Waterworks Yard
	Inset balconies		Pall Mall warehouse
	Landmark building		Community/culture building
	SuDS		



Fig 101 Canal Square & City College Character Area

4.4.2 Movement

Vauxhall Road

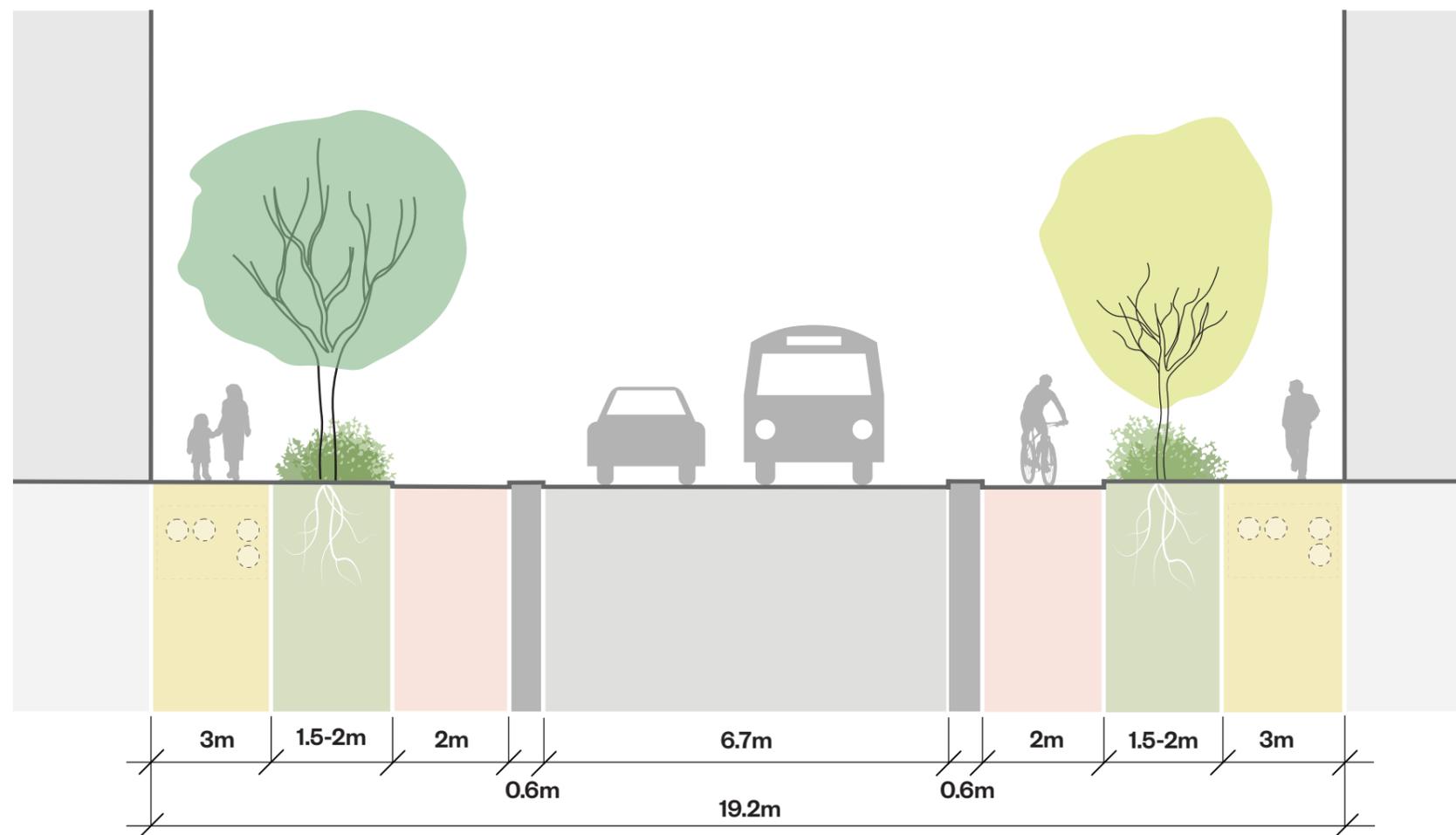
- » Vauxhall Road will retain its role as a primary route into and out of the city centre. Its design will support active travel through the introduction of segregated cycle lanes, wider pavements, and new tree planting.
- » Loading and unloading bays will be provided along Vauxhall Road, but on-street parking will not be permitted.

Eaton Street

- » Eaton Street will be transformed into an emergency and servicing-only street, prioritising pedestrian movement between Vauxhall Road, Canal Square, and Pall Mall beyond.
- » The existing cobbles will be restored, reinforcing the historic character of this route.



Fig 102 Vauxhall Road today



KEY

- Carriageway
- Footpath with underground service corridor
- Cycle Lane
- Tree/planting zone
(underground cells will need extending under pavement to provide a 2.5m width to tree pit)

Fig 103 Vauxhall Road proposed section

Gascoyne Street

- » Gascoyne Street will become a pedestrian-only route. It will remain a public route improving east-west permeability. It will feature restored cobbles and planting to prevent vehicular movement.

Pumpfields Road

- » Pumpfields Road is the main local street serving this character area.

- » It will feature continuous pavements on both sides and tree planting along its full length.
- » Street design will reduce traffic speeds and flows to create an environment where people feel safe to cycle in the carriageway.
- » Parallel street parking will be provided, and a new raised table will be introduced where Pumpfields Road borders the new Canal Square, offering pedestrian priority and traffic calming.

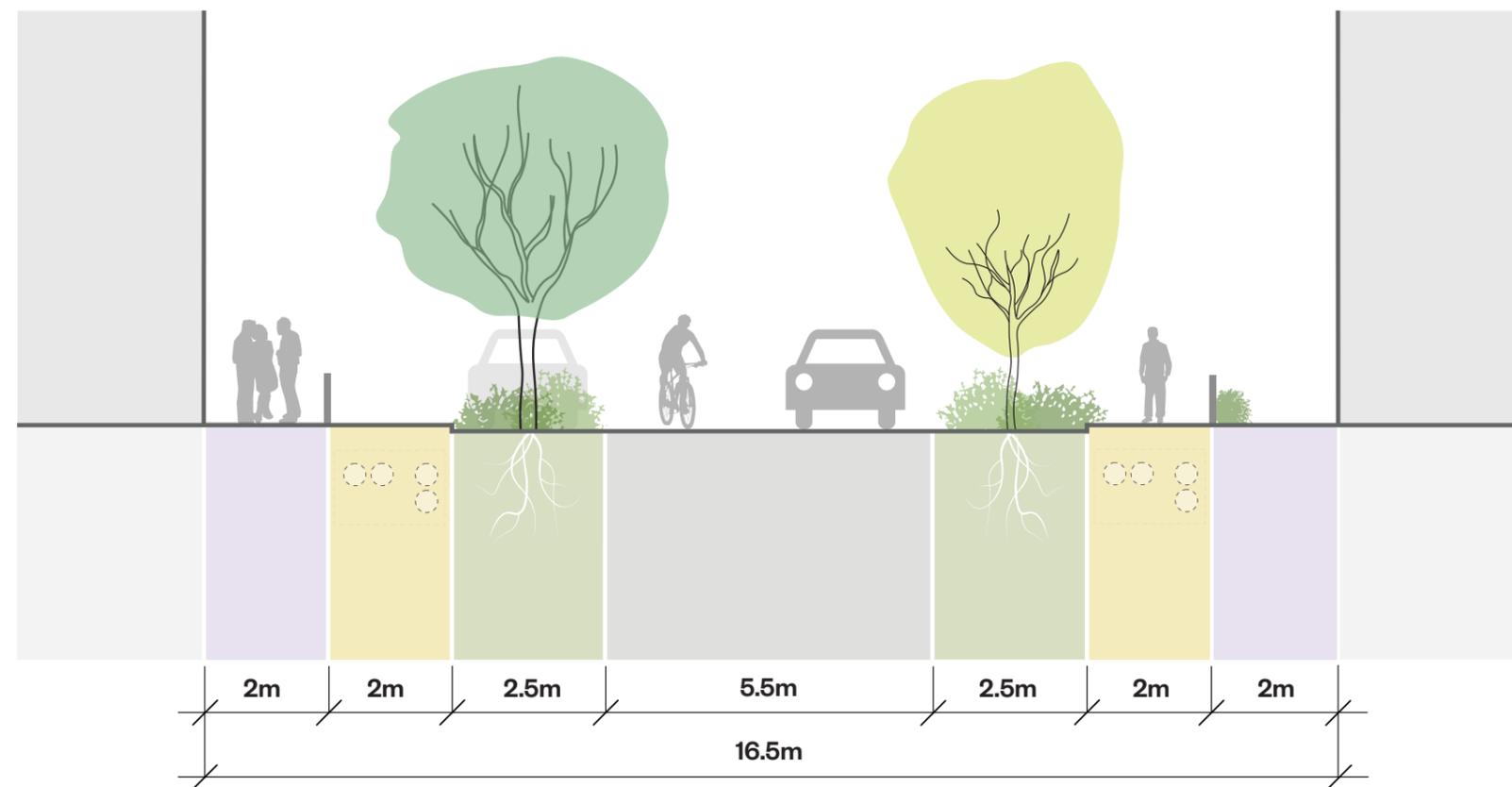


Fig 105 Pumpfields Road proposed section



Fig 104 Pumpfields Road today

KEY

- Carriageway
- Footpath with underground service corridor
- Tree/planting zone with on street parking
- Building zone

Pall Mall

- » The Pall Mall carriageway will be reduced to 5.5 metres to reflect its secondary role compared to Vauxhall Road.
- » The space gained will be dedicated to wider pavements, a segregated cycle lane, and a wide planted swale running the entire length of the road, further reinforcing the historic canal route that once ran north-south through this area.

Spatial guidance: Movement

- a. Development proposals within the Canal Square & City College Character Area must demonstrate how they respond to and contribute to the design principles set out in this SPD for Vauxhall Road, Pumpfields Road, Eaton Street, and Gascoyne Street.

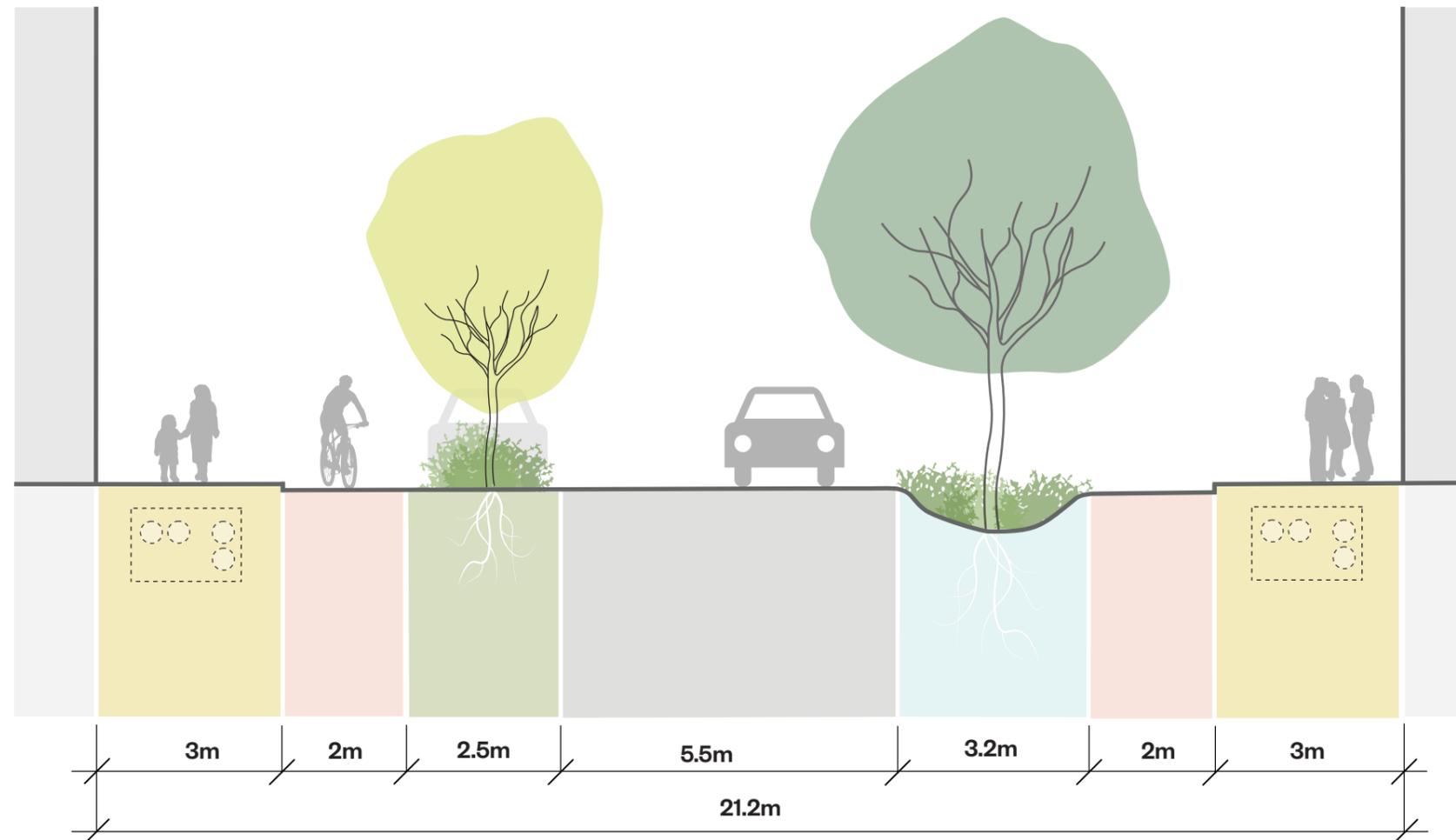


Fig 107 Pall Mall proposed section



Fig 106 Pall Mall today

KEY

- Carriageway
- Footpath with underground service corridor
- Cycle Lane
- Tree/planting zone with on street parking
- Planted swale

4.4.3 Public space and nature

Canal Square

- » Canal Square occupies a location of profound historical significance, where a cluster of retained heritage structures - the Pall Mall warehouse and historic boundary walls - reflect the area's rich cultural and architectural legacy.
- » This site also marks the historic terminus of the Leeds & Liverpool Canal, which defined the area's character for centuries until its course was altered during the 1980s construction of the Eldonian Village.
- » The Square presents a unique opportunity to reinforce and celebrate the site's heritage through the delivery of a distinctive and inclusive new public space.
- » Canal Square is envisaged as a civic heart, acting as a catalyst for regeneration and offering vital open space relief amid the anticipated higher-density developments within the Tall Buildings zone along Leeds Street and Great Howard Street.
- » Canal Square will be a flexible, multi-functional civic space that accommodates local retail, informal gathering, and a wide range of community and cultural events;
- » The character of Canal Square will be a celebration of heritage, retaining and integrating the existing Pall Mall warehouse walls which help define the square's edges and create smaller, more intimate sub-spaces. A new linear water feature will mark the historic alignment of the Leeds & Liverpool Canal.
- » The square will be lined with non-residential ground-floor uses providing active frontages, and enhanced by high-quality tree planting and lighting to support the day- and night-time economy.
- » The square will feature two new pavilion buildings sensitively located within the existing heritage walls, intended to house community and educational uses.

Spatial guidance: Canal Square

- a. The existing heritage walls must be fully retained and their setting enhanced through sensitive design. These historic structures should form a defining framework for the square, contributing to its identity and character.
- b. A water feature must be incorporated into the square, aligned with the historic route of the Leeds & Liverpool Canal, to evoke the site's industrial past and create a strong narrative connection to local history.
- c. Non-residential uses must be clustered around the square to ensure a continuous and lively active frontage at ground floor level. Uses should include community, cultural, food & beverage, and retail supporting both daytime and night-time economy activity.
- d. The square must include tree planting, high-quality seating, cycle parking and SuDS features (e.g. linear rain gardens) integrated into the public realm design. The layout should include shaded areas to promote comfort and encourage rest and social interaction throughout the day and seasons.





Canal Square will become the civic heart of the SPD - a vibrant, mixed-use destination where education, culture, community, and commercial converge, seamlessly integrating heritage and new homes to serve local neighbourhoods and draw visitors from the city centre.



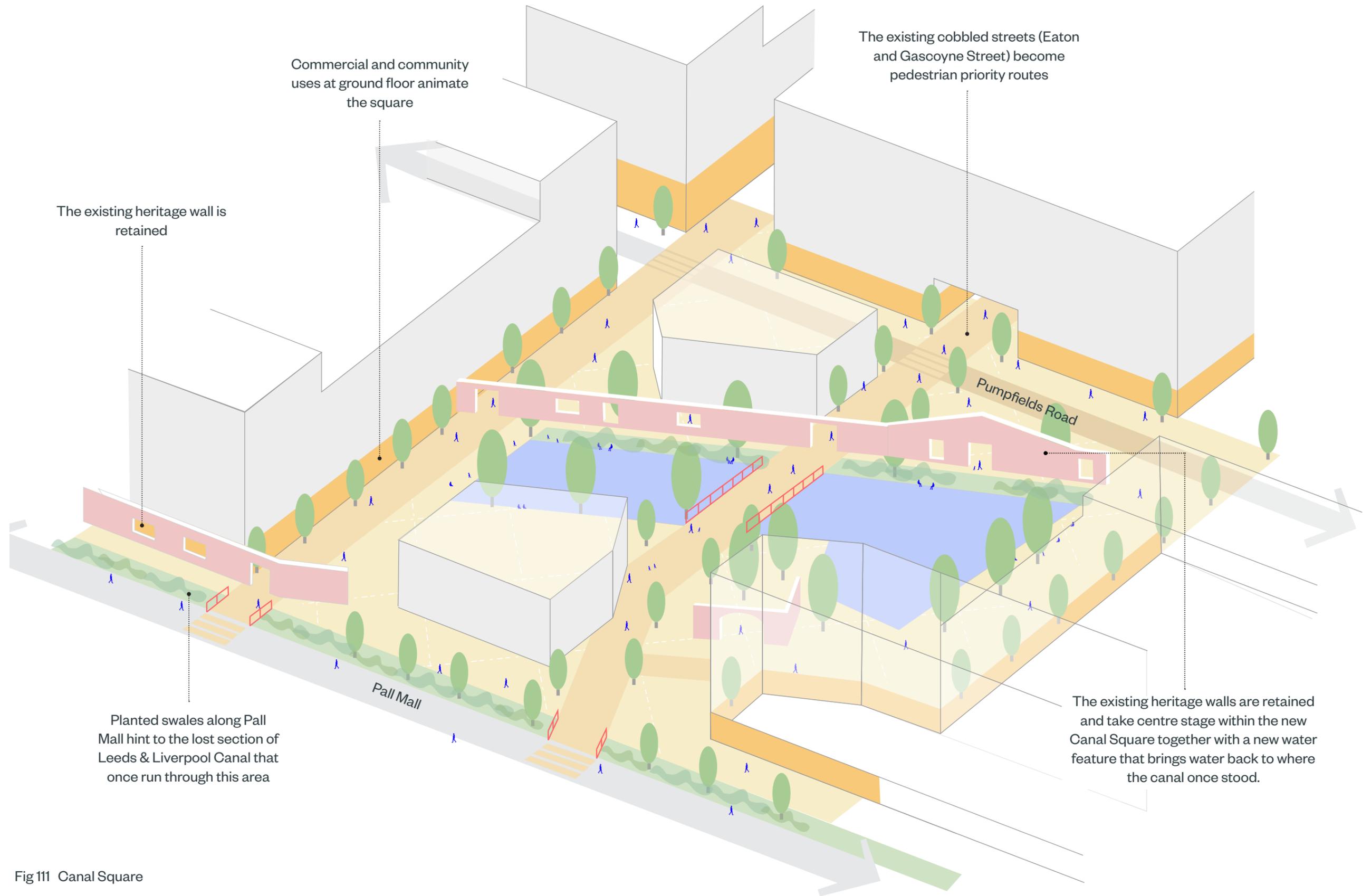
Fig 108 Example of water feature for Canal Square



Fig 109 Example of planted swales for Pall Mall



Fig 110 Example of water feature for Canal Square



Commercial and community uses at ground floor animate the square

The existing cobbled streets (Eaton and Gascoyne Street) become pedestrian priority routes

The existing heritage wall is retained

Pumpfields Road

Pall Mall

Planted swales along Pall Mall hint to the lost section of Leeds & Liverpool Canal that once run through this area

The existing heritage walls are retained and take centre stage within the new Canal Square together with a new water feature that brings water back to where the canal once stood.

Fig 111 Canal Square

Vauxhall Square and Waterworks Yard

- » Vauxhall Square is a new public space which improves the setting of the City College within the area and its relation with Vauxhall Road.
- » The public realm fronting the City College with planting, seating and lighting will provide a welcoming and safe entrance space for students and visitors.
- » Tree planting within Vauxhall Square is intensified providing a buffer from the noise and increasing the feeling of protection from the road.
- » The design proposals also aim to reactivate the College's frontage along Pumpfield Road by opening up new entrances into the college building which will help reactivating the frontage.
- » The Indicative Masterplan also identifies the potential opportunity to open up City College's working yard, Waterworks Yard, to the public.
- » Waterworks Yard, named after a historic structure that once stood in the area, will become a new public space, helping the College re-establish a connection with Pall Mall, the western part of the masterplan, and providing a direct link to Canal Square.
- » The existing college shed could be repurposed as a canopy, offering shade and shelter for activities in all kinds of weather.

Spatial guidance: Vauxhall Square and Waterworks Yard

- a. Vauxhall Square should incorporate seating and lighting to create a welcoming environment where students and visitors can socialise and relax.
- b. Tree planting and landscaping should be intensified to form a natural buffer against traffic noise and contribute to improved air quality.
- c. To ensure the space remains active and vibrant throughout the day, Vauxhall Square must be fronted by non-residential uses, such as retail, community, or leisure functions, that support street-level activation.
- d. Design proposals should incorporate a new entrance and public space along Pumpfields Road, (i.e. Waterworks Yard), establishing links to Pall Mall and Canal Square.



Fig 112 Example of tree planting and seating for Vauxhall Square

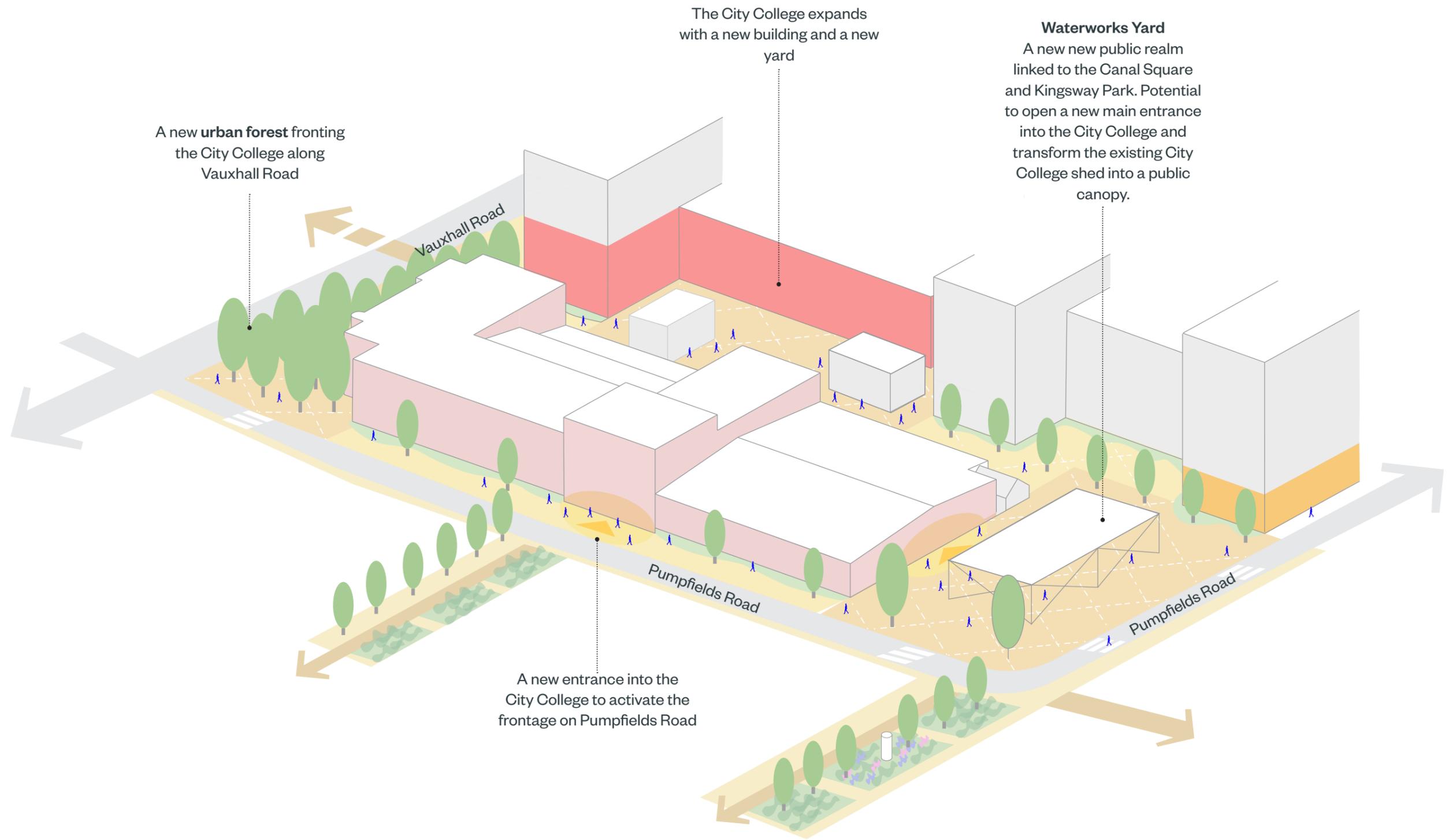


Fig 113 Vauxhall City College

4.4.4 Built form

- » Buildings within the Canal Square will embody a civic quality that fosters community and public engagement, akin to public or civic buildings.
- » This civic quality will feature generous floor-to-ceiling heights at ground floor, large apertures, and well-defined entrances that serve as focal points and encourage interaction.
- » An integrated ground floor design that seamlessly blends the public realm with commercial spaces could be achieved through the use of a colonnade.

Spatial guidance: Built form

- a. The built form should respond positively to the setting of the non-designated heritage assets.
- b. Apartment buildings should maximise the number of dual-aspect dwellings, avoid single-aspect north-facing units, minimise internal corridor lengths, and ensure high standards of natural light and ventilation.
- c. Both residential and non-residential frontages should promote passive surveillance and contribute to a safe and welcoming public realm.
- d. Building heights in this area should range from five to six storeys, rising to ten or eleven storeys along Vauxhall Road, which falls within the Leeds Street Tall Buildings Cluster.
- e. Façade layouts should follow a strong and regular grid. Highly expressive elements—such as columns, varied textures, and material finishes—should be incorporated to add visual depth and tactile interest.
- f. The overall façade design should feel cohesive in colour and tone, yet rich in texture and depth.
- g. Balconies along Vauxhall Road and Pall Mall must be inset. Around the square, a mix of inset and projecting balconies is encouraged.
- h. Roofscape design should take cues from the area's character and industrial heritage. Pitched, sawtooth, and flat roofs are acceptable, while mansard roofs must be avoided.
- i. The two pavilion buildings within the square must be of high architectural quality. They should feature large apertures, and their materials should distinguish them from the surrounding residential buildings while drawing on the area's industrial character. The use of metal, for example, is encouraged.



Pumpfields Road will become a green, people-friendly local street, designed with continuous pavements, tree planting, and traffic calming to encourage safe cycling, supported by a raised table crossing at Canal Square.



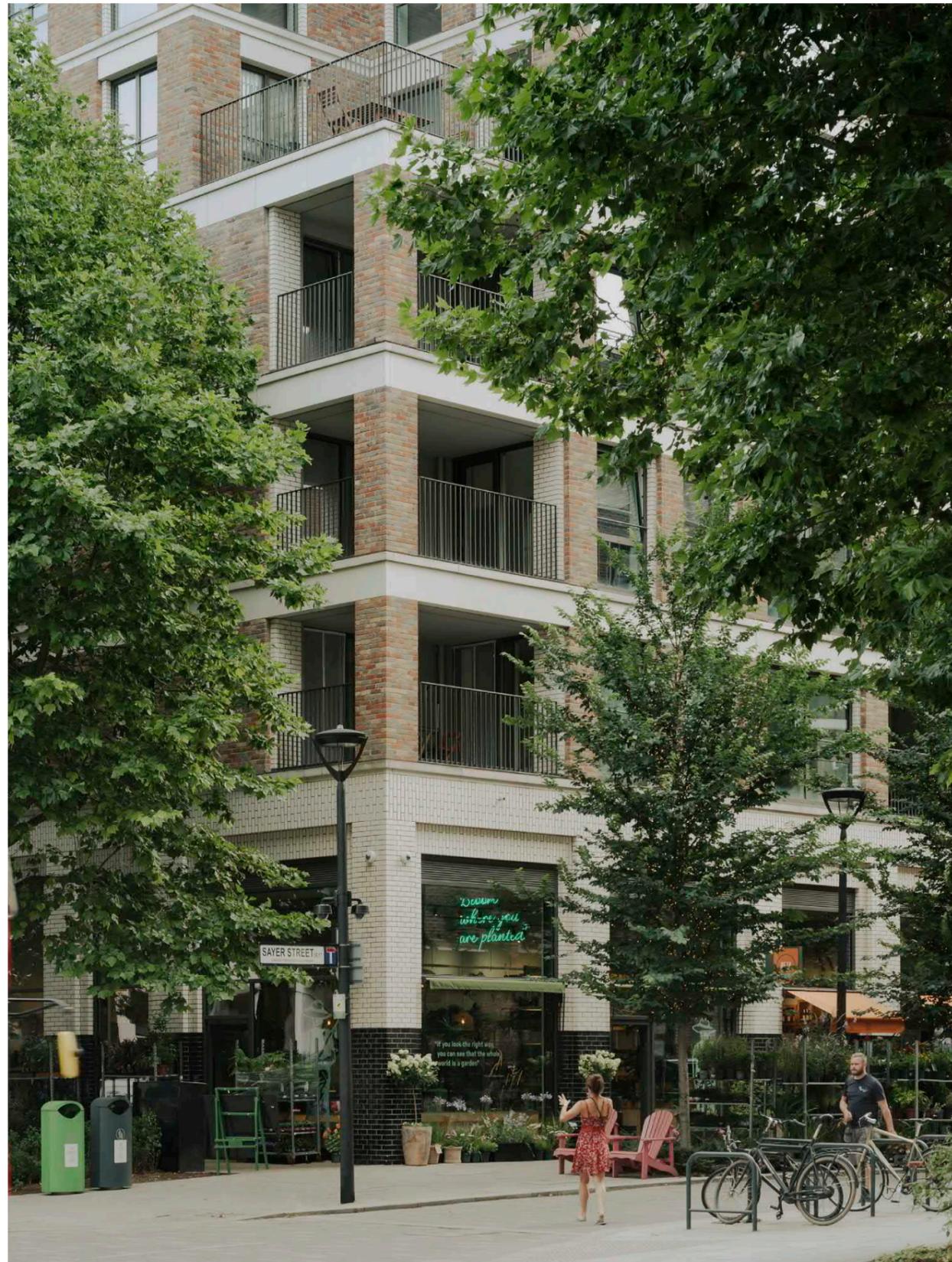


Fig 114 Apartment building with commercial uses at ground floor



Fig 115 Retained warehouse building with contemporary extension accommodating community uses

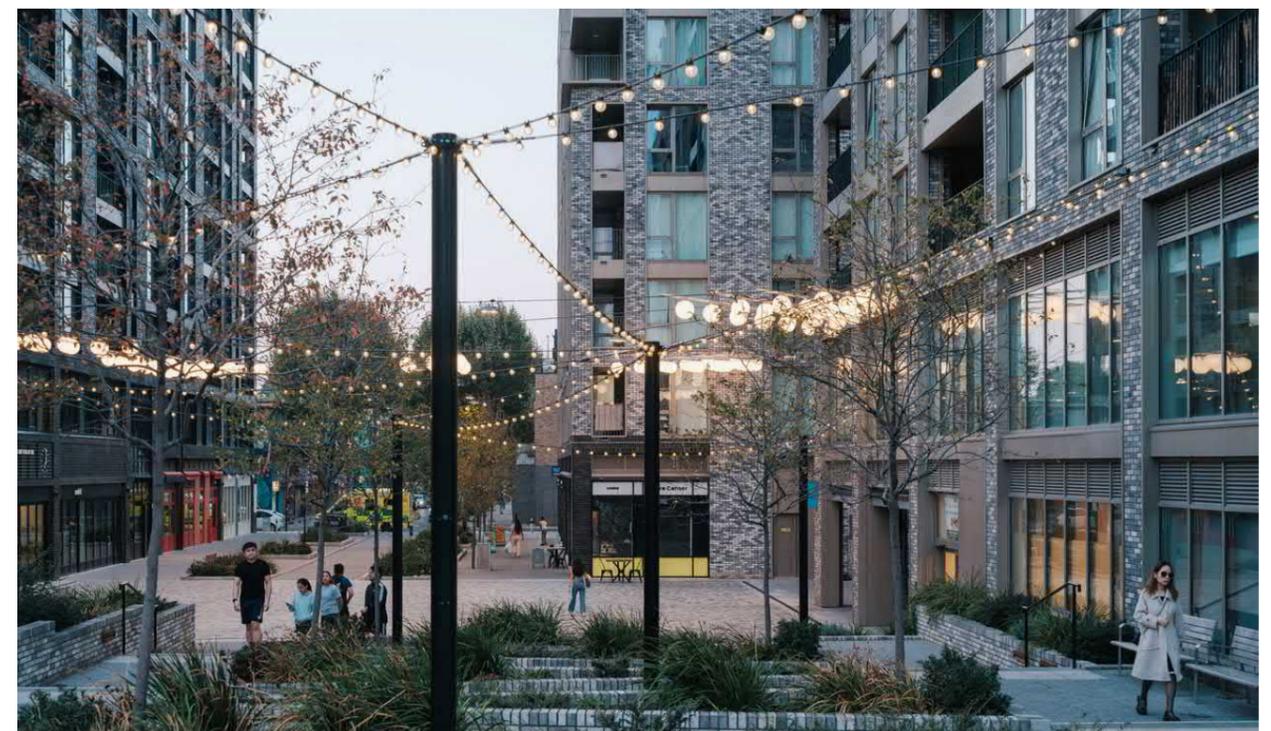


Fig 116 Public square and lighting activated by non-residential ground floors



Vauxhall Road will be transformed into a green and welcoming gateway to the city centre, prioritising active travel through segregated cycle lanes, wider pavements, and tree planting, with vibrant commercial ground floors bringing the street to life.

4.4.5 Uses

- a. The Canal Square and City College area will be a residential-led mixed-use neighbourhood comprising educational, community, cultural, commercial, and employment uses.
- b. Education will remain the primary focus of this area, centred around the existing City College and its potential expansion into a new mixed-use building to accommodate the forecasted increase in the student population.

Spatial guidance : Uses

- a. Development proposals should incorporate employment uses at ground floor level along the east-west section of Pumpfields Road, from Vauxhall Road to Pall Mall.
- b. Commercial uses must be provided along the north-south section of Pumpfields Road and clustered around Canal Square.
- c. The two pavilion buildings within Canal Square should accommodate community and cultural uses, e.g. a library and study rooms, reinforcing the area's strong educational role and its relationship with City College and the growing student population.



Fig 117 Ground floor activated with employment uses and workshops.

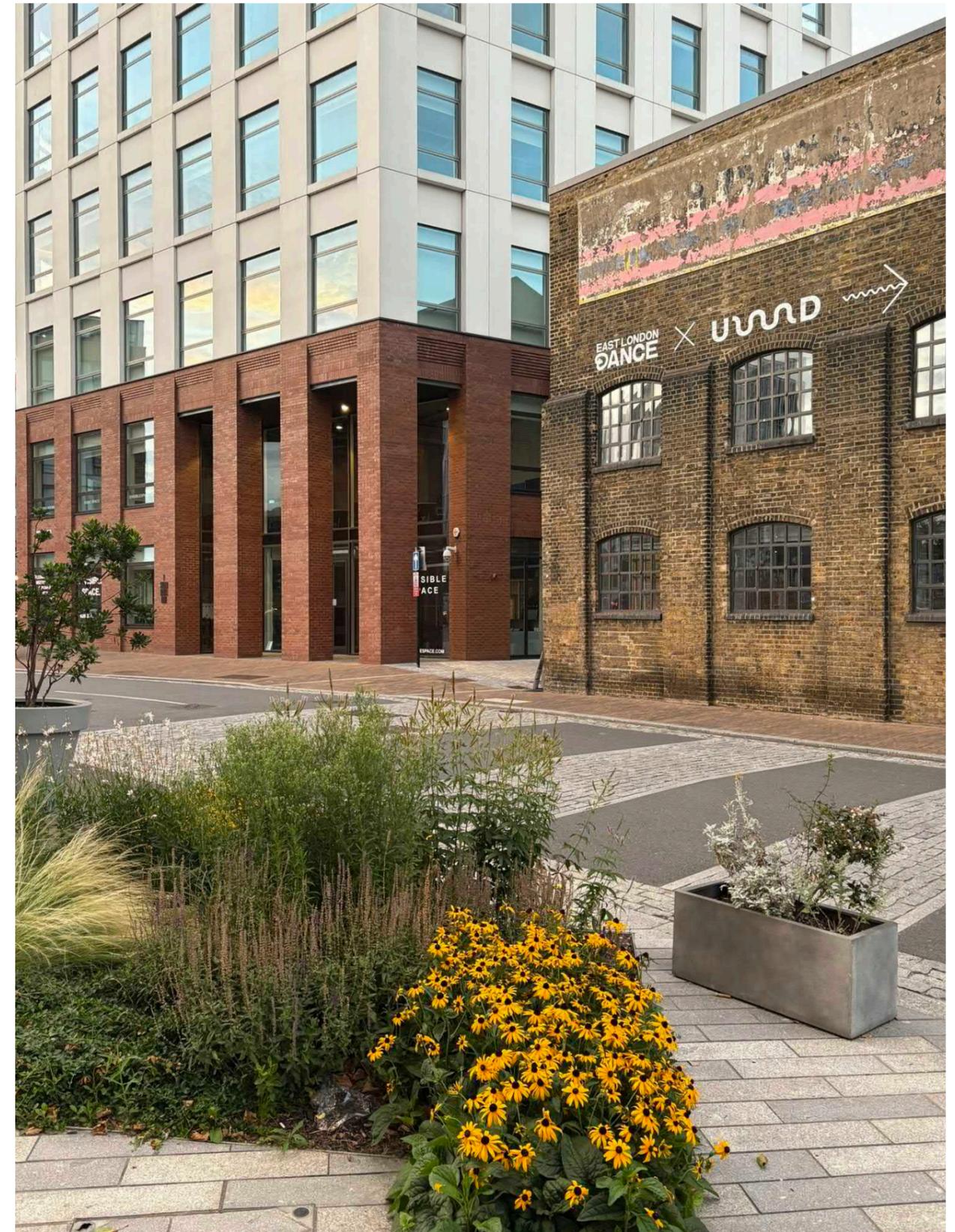


Fig 118 The façade design of the new mixed-use building takes cues from the adjacent retained warehouse, incorporating a two-storey base with a rhythm defined by piers.

4.5 City Centre fringe & gateway

4.5.1 Vision & identity

- » The City Centre fringe and gateway area lies along the major routes of Leeds Street and Scotland Road.
- » This area will form a threshold between Pumpfields and Limekilns and the inner core of the city centre. As such, it is where the majority of larger and taller buildings are expected to be located.
- » Development along these key roads will serve to re-activate the frontage, while also maximising density with prominent landmark buildings. These buildings will play a key role in announcing the gateways into the city centre, particularly for those arriving from the northern residential areas.
- » The Vision indicates what could be possible should sites become available for redevelopment.

KEY

- | | | | |
|---|--------------------------|---|--|
|  | SPD boundary | ① | Waterloo Gardens |
|  | Heritage assets | ② | YMCA building |
|  | SPD proposed buildings | ③ | Pedestrianised Leeds St/
Great Howard Str junction
(Waterfront Vision) |
|  | Non-residential frontage | ④ | Pall Mall SPD |
|  | Inset balconies | ⑤ | Limekiln Lane Bridge |
|  | Landmark building | ⑥ | Unite Student Accommod. |
|  | SuDS | | |

Spatial guidance : Vision & identity

- a. Development proposals within the City Centre fringe & gateway Character Area must demonstrate how they contribute to the vision for the area set out in this SPD.

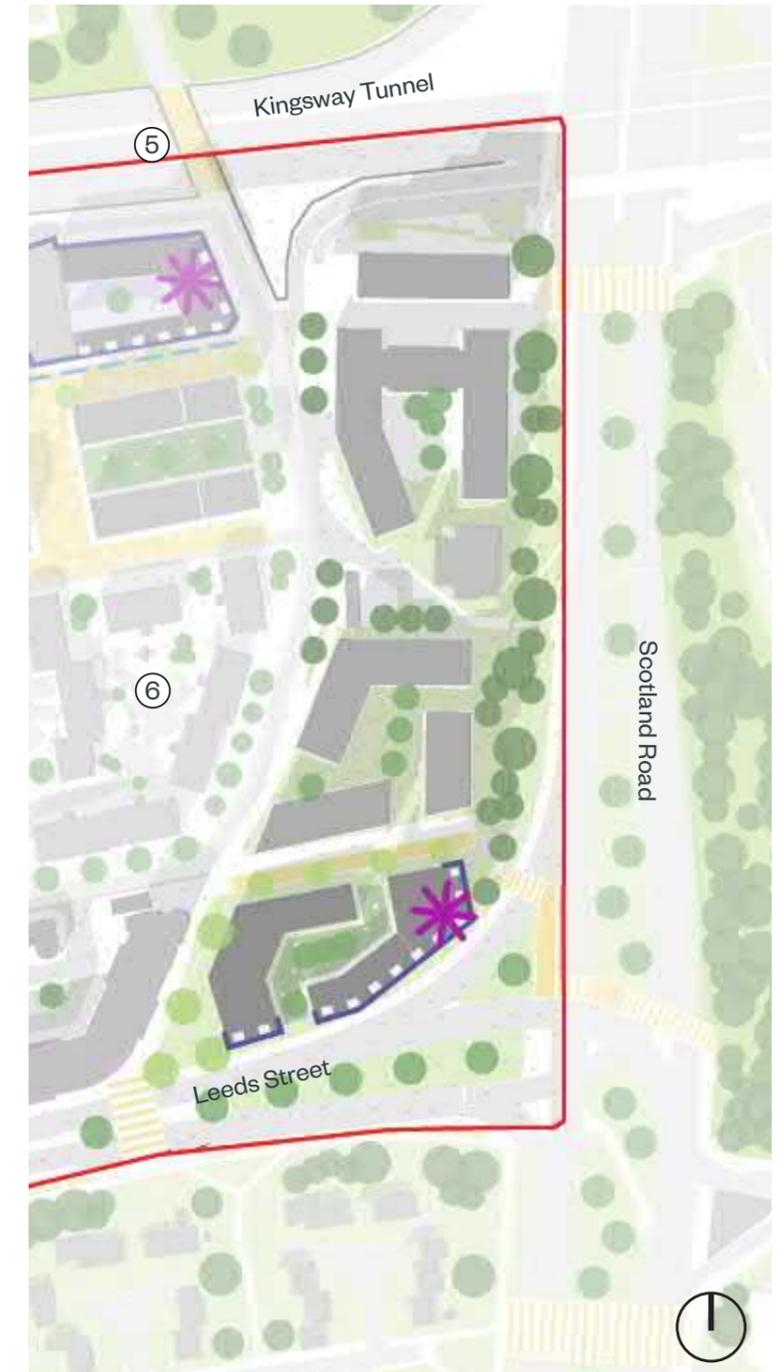


Fig 119 City Centre fringe & gateway Character Area

4.5.2 Movement

Leeds Street

- » Leeds Street will be transformed into a vibrant city centre boulevard, reactivated through non-residential ground floor uses which, together with improved crossings and widened footways, will help address the severance caused by high traffic flows.
- » While it will continue to serve as a key east-west artery around the inner core of the city centre, its character will shift towards an active travel corridor, prioritising walking, wheeling, and cycling, alongside enhanced public transport routes.
- » Comprehensive tree planting and the integration of Sustainable Drainage Systems (SuDS) will support climate resilience, providing shade during hotter periods, mitigating the impact of

heavy rainfall through sustainable stormwater management and providing a buffer from noise and pollution caused by vehicular traffic.

- » No on-street parking will be allowed along its entire route.

Scotland Road & Great Howard Street

- » Active travel, tree planting, and Sustainable Drainage Systems (SuDS) will be enhanced along both Scotland Road and Great Howard Street. These routes will be reactivated through non-residential uses at ground floor level, contributing to a more vibrant and welcoming streetscape.
- » In line with the Waterfront Vision SPD, a significant section of

Great Howard Street, including its junction with Leeds Street, will be transformed through a cut-and-cover solution, taking the road underground and allowing the junction to be fully pedestrianised. This transformation will further strengthen pedestrian, wheeling and cycling connections between Pumpfields & Limekilns and the waterfront. As studies and partnerships progress for this major infrastructure project, a temporary surface-level intersection redesign could test and validate concepts while providing short-term benefits before the longer-term cut-and-cover scheme is delivered.

Spatial guidance: Movement

- a. Development proposals within the City Centre fringe & gateway Character Area must demonstrate how they respond to and contribute to the design principles set out in this SPD for Leeds Street, Scotland Road and Great Howard Street.

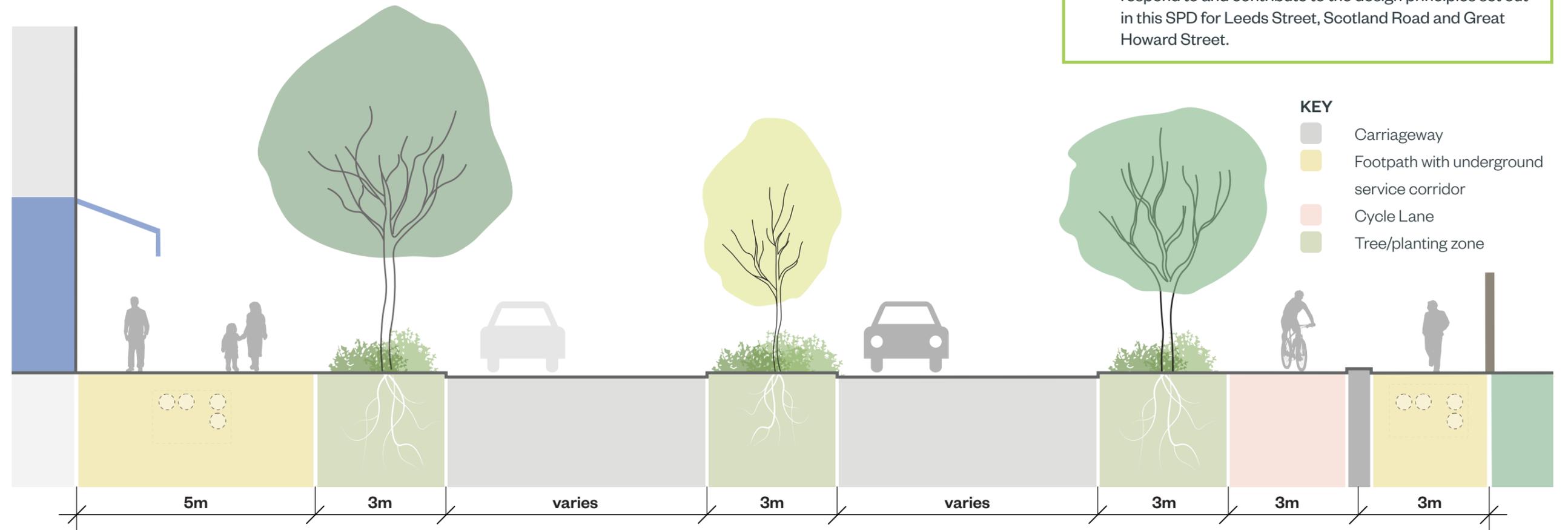


Fig 120 Proposed Leeds Street section

4.5.3 Public space and nature

Waterloo Gardens

- » Waterloo Gardens is located above the Merseyrail tunnel and will serve as a terminus of the green spine emerging within the Exchange Station site south of Leeds Street, linking Pumpfields and Limekilns SPD with the emerging Pall Mall SPD and the wider city centre.
- » Open to the south, the gardens will maximise daylight and sunlight between larger, taller buildings and will act as a stepping stone along Leeds Street from Canal Square to the cut-and-cover of the Leeds Street/Great Howard Street junction, which will lead to the pedestrianisation of the intersection.
- » Design features within the gardens (such as paving, furniture, sculptures) will reference the rich history of the area, once a thriving hub for trade and passengers with both Waterloo Station and Exchange Station.

Spatial guidance: Waterloo Gardens

- a. Design proposals should carefully consider wind impacts associated with surrounding tall buildings, incorporating mitigation measures where necessary. These measures should be integrated into the public realm through elements such as planters, trees, and other landscape features.
- b. The design of the gardens should draw inspiration from the heritage and history of Waterloo Station and the wider area.
- c. Waterloo Gardens must be activated by non-residential uses at ground floor and must include visitor cycle parking.

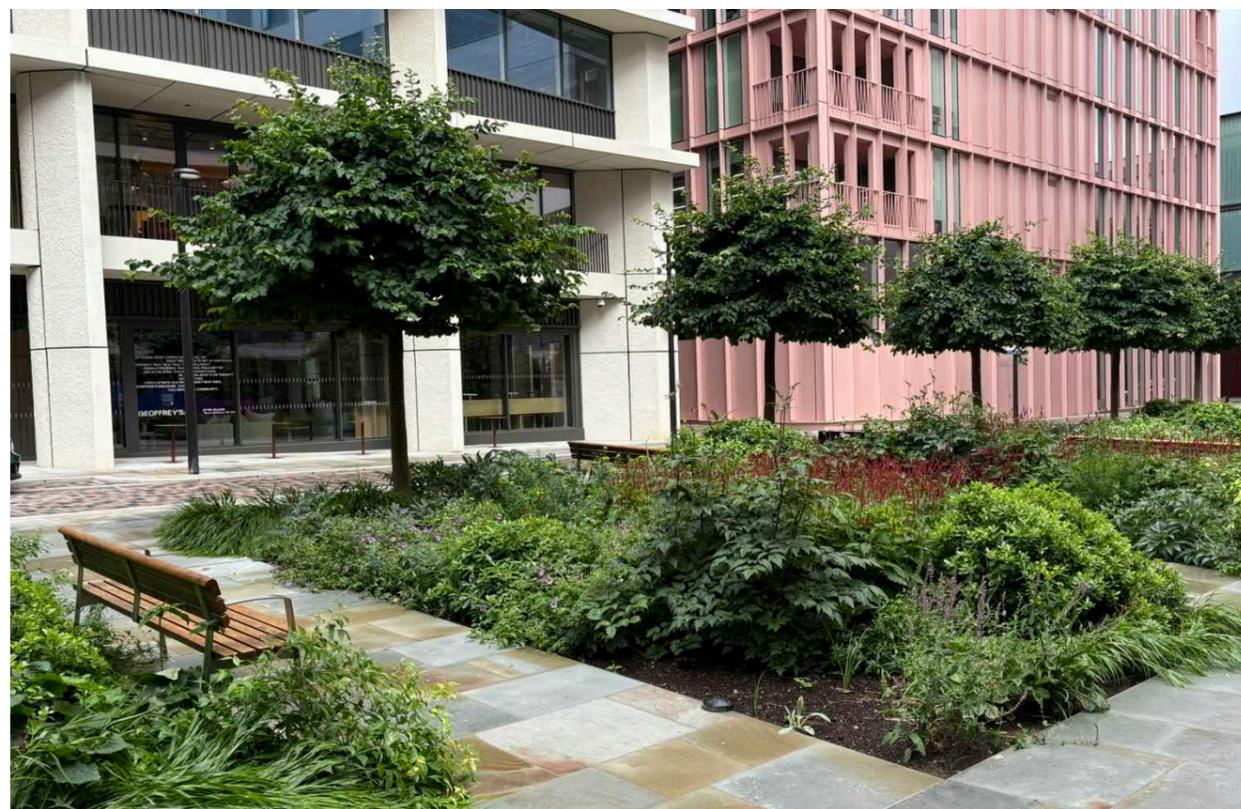


Fig 121 A small-scale public realm, featuring trees and integrated seating, is framed by larger buildings with active non-residential ground floor uses



Fig 122 Public realm incorporating integrated artwork that celebrates the site's history and heritage.

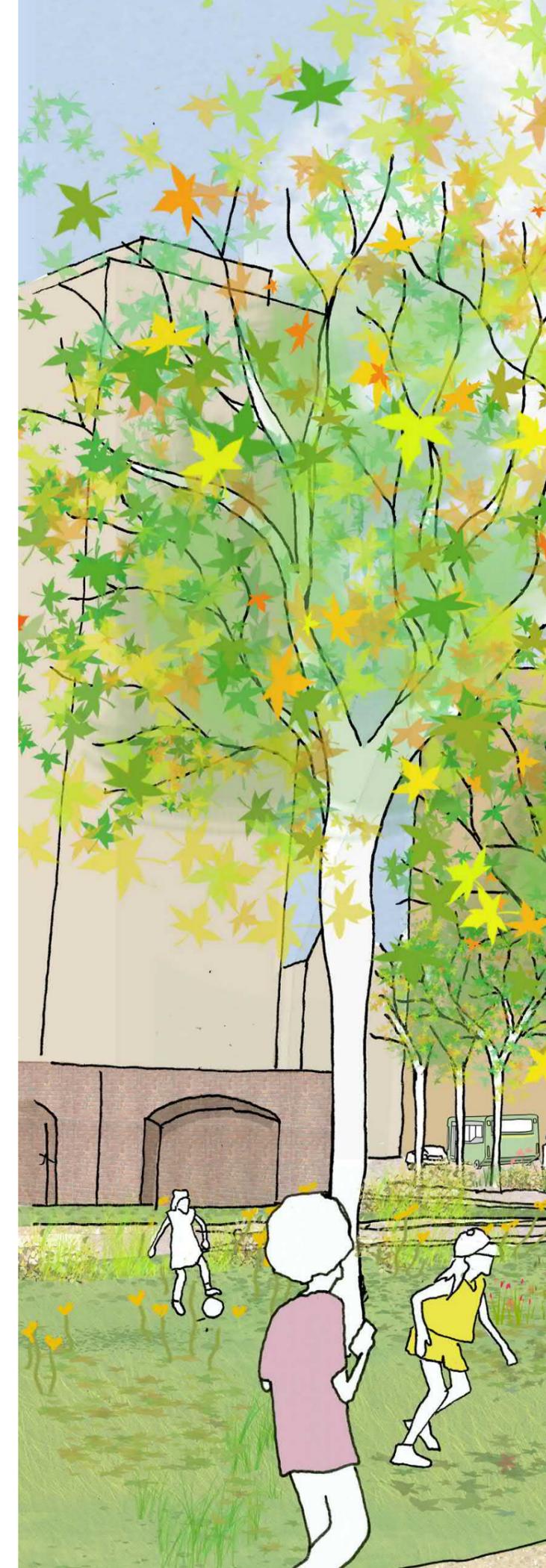
4.5.4 Built form

- » The City Centre Fringe and Gateway area will accommodate the largest and tallest buildings within the SPD, reflecting the location of the Tall Buildings Cluster Zones that fall within this character area.
- » The tallest building will be a 28-storey tower at the junction of Leeds Street and Great Howard Street, followed by 18-storey and 13-storey towers along Leeds Street.
- » The towers will be arranged in courtyard typologies, allowing for the creation of communal courtyards that are sheltered from the noise and pollution of the major roads.
- » Dwelling types within this character area will predominantly consist of apartments, with potential for maisonettes located away from the major roads.

Spatial guidance: Built form

- a. Building heights should range from a 28-storey tower at the Leeds Street / Great Howard Street junction to 6–7 storeys.
- b. The overall form and silhouette of tall buildings should have a distinctive yet carefully considered identity, contributing positively to the sense of place.
- c. Key View 1 - Magazine Promenade toward Metropolitan Cathedral must be protected.
- d. Building form must respond to wind mitigation and microclimate considerations.

- e. Tall buildings must articulate a base or plinth of one to two storeys that follows the street level. This base should feature a diverse and layered design with active frontages and clearly defined openings.
- f. The base of the building should be articulated through a change in material or façade treatment. It may also be defined by a setback.
- g. Main entrances should be strategically located and aligned with the vertical articulation of the façade to aid legibility and wayfinding.
- h. Wind mitigation must be considered early in the design process and addressed through massing and façade articulation. The design should respond to the impact of microclimatic conditions, including overshadowing and downdraughts.
- i. Where wind mitigation cannot be addressed through the building envelope, interventions should be integrated into the public realm design, using planters, trees, or other landscape features, to reduce wind impacts around tall buildings.
- j. The roofs of tall and landmark buildings should continue the architectural language of the façade to form a unified and coherent whole.
- k. Articulation of roof forms may be achieved through an inset, provided it maintains consistency with the façade design.
- l. Any articulation at roof level should be a minimum of two storeys in height.





Leeds Street will be redefined as a climate-resilient, people-focused boulevard - balancing its role as a key transport artery with vibrant ground floor activity, enhanced public transport, and a generous public realm that prioritises walking, cycling, and green infrastructure.



Fig 123 Tall building elevation with a well articulated base, middle and top, providing a balance of proportion between each part



Fig 124 Tall building along a railway line



Fig 125 Sunlight and daylight are maximised within the residential courtyard through strategic orientation and massing.

4.5.5 Uses

- » The City Centre Fringe and Gateway will introduce new commercial uses at ground floor level, reactivating frontages along the major routes surrounding the site. This will help strengthen ties with the land uses within the City Centre inner core and reduce the severance caused by these major routes.

Spatial guidance: Uses

- a. Development proposals should incorporate commercial uses at ground floor level along Leeds Street, Scotland Road, and Great Howard Street.
- b. Proposals should also include a tall building for commercial and employment uses at the junction of Leeds Street and Great Howard Street, responding to the character and land uses of the Business District south of Leeds Street.



Fig 126 Public-facing non-residential ground floor uses encourage visual connection and overlooking onto the public realm.



Fig 127 The ground floor's non-residential use is expressed through a change in brick colour, creating visual distinction

4.6 Railway edge

4.6.1 Vision & identity

- » The Railway edge character area is located between the railway tracks and Great Howard Street, Pall Mall, and Love Lane.
- » Development in this area will play a key role in reactivating the railway arches, acting as a hinge between Pumpfields & Limekilns, the Ten Streets area, and the waterfront beyond.
- » It will provide opportunities for both new and existing businesses, while also enhancing east-west permeability for walking, wheeling and cycling.

Spatial guidance: Vision & Identity

- a. Development proposals within the Railway Edge Character Area must demonstrate how they contribute to the vision for the area set out in this SPD.

KEY			
	SPD boundary		Grand Arch
	Ten Streets SPD		Pall Mall warehouse
	Heritage assets		Pedestrianised Leeds St/ Great Howard St junction (Waterfront Vision)
	SPD proposed buildings		Mobility Hub
	Non-residential frontage		Eldonian Village
	Inset balconies		Arches Yard
	Landmark building		Burlington St (cycle route)
	SuDS		



Fig 128 Railway Edge Character Area

4.6.2 Movement

Mobility Hub

- » A new mobility hub is proposed at the junction of Great Howard Street and Chadwick Street. The hub is intended to support LCC's parking strategy, whilst limiting vehicular movements within the SPD area. The hub will also support transfer between modes including bus services along Great Howard Street, the potential BRT service, and active travel movements east-west between the waterfront, Ten Streets and SPD area.
- » Strategically located at the intersection of key north-south and east-west movement corridors, the hub will capture vehicular trips to the town centre, whilst providing access to active travel routes, and bus services adjacent to and within the SPD area.
- » Provision or parking in this location for the public and for residents seeks to limit vehicular trips into the city centre and

traffic flows within the SPD area, supporting healthier, people-centric streets.

- » The mobility hub will offer a range of integrated facilities, including:
 - Secure bicycle parking with showers and changing rooms
 - Spaces for club cars and car share schemes
 - Charging points for electric bikes and cars
 - Car parking for residents and visitors
 - A parcel delivery hub and secure parcel lockers
 - A public rooftop with views towards the River Mersey
- » Designed to a high architectural standard, the hub will be adaptable and future-ready, capable of reuse, disassembly, or conversion to alternative uses as mobility needs evolve.

Spatial guidance: Movement

- a. The mobility hub must be designed to a high architectural standard, contributing positively to the surrounding built environment. Flexibility must be a core principle of its design, ensuring the structure can adapt over time as mobility patterns and community needs evolve.
- b. The mobility hub must include cycle parking, car parking for residents and visitors, parcel delivery hub, club car spaces and must be designed in close collaboration with the Council's Highways and Active Travel teams.
- c. The mobility hub should strengthen connections to the wider cycling network and public transport systems.



Fig 129 Mobility Hub set within a landscape buffer with tree planting and integrated seating



Fig 130 Mobility Hub designed with flexibility in mind, allowing it to adapt to future mobility and community needs

Chadwick Street

- » Vehicular movement along Chadwick Street will be restricted to a single railway arch, with the adjacent arch dedicated exclusively to walking, wheeling, and cycling.
- » This intervention will create a key east-west active travel link between Pumpfields and Limekilns, the Ten Streets SPD area, and the waterfront beyond.

- » Chadwick Street will also serve as an important connector between the proposed mobility hub on Great Howard Street and the segregated cycle route along Pall Mall.
- » This connection will support seamless modal shifts for those travelling to and from the city centre, enhancing the integration of sustainable transport options across the area.

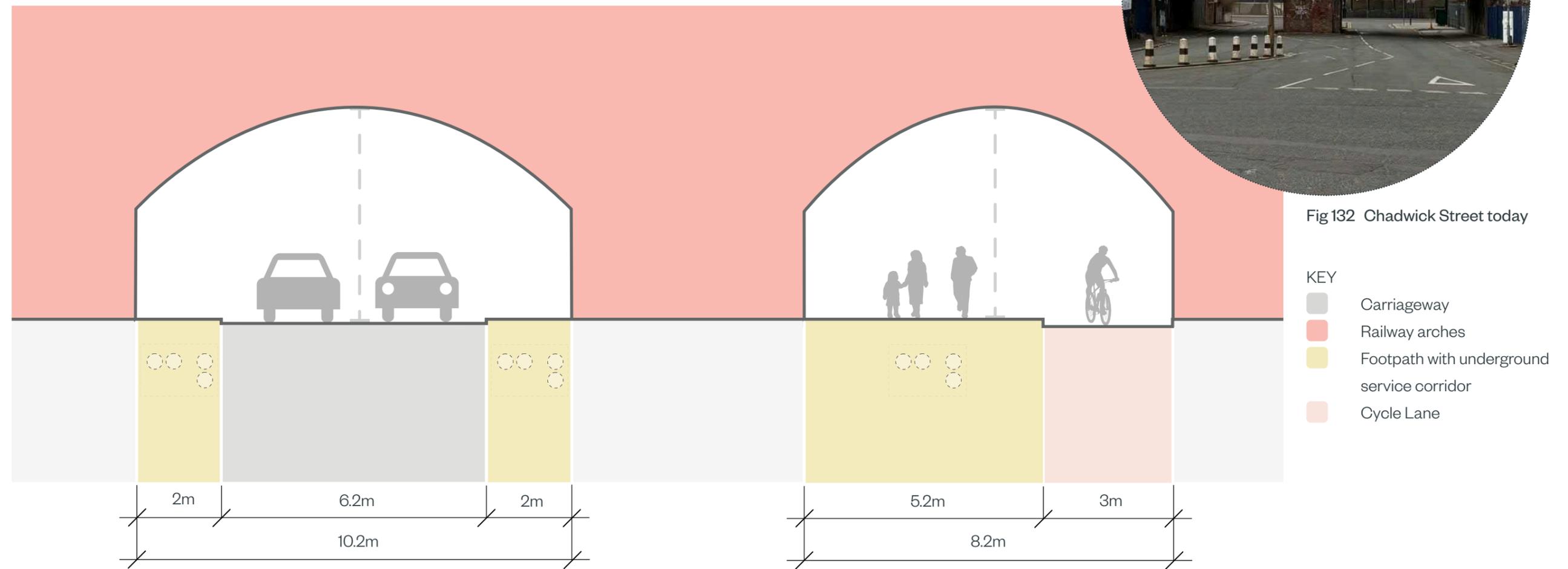


Fig 132 Chadwick Street today

Fig 131 Chadwick Street proposed section

Love Lane

- » Love Lane will be reinforced as a key active travel route into the Ten Streets area. Pavements will be introduced on both sides of the carriageway, complemented by tree planting to provide shade and improve the overall streetscape quality.
- » Wider pavements along the railway arches will create opportunities for non-residential uses within the arches to spill out into the public realm, encouraging activity and vibrancy at street level.
- » Road geometry and traffic calming measures will ensure that

conditions are suitable for cycling in carriageway.

- » On-street parallel parking will be integrated alongside the trees to ensure parked cars do not dominate the streetscape.

Spatial guidance: Movement

- d. Development proposals within the Railway Edge Character Area must demonstrate how they respond to and contribute to the design principles set out in this SPD for Chadwick Street and Love Lane.



Fig 133 Love Lane today

KEY

- Carriageway
- Footpath with underground service corridor
- Cycle Lane
- Tree/planting zone with on street parking
- Planted swale

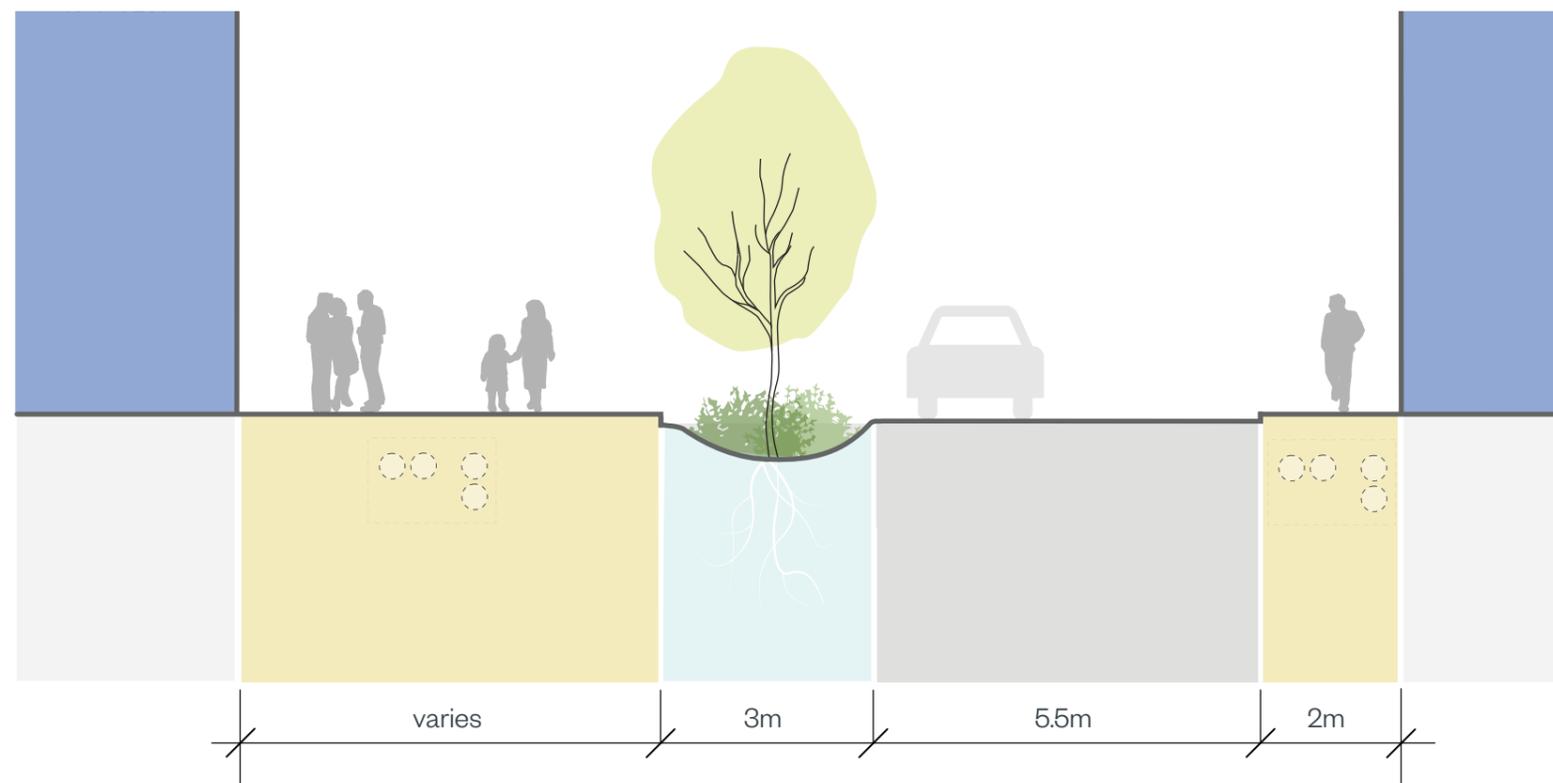


Fig 134 Love Lane proposed section

4.6.3 Public space and nature

The Grand Arch

- » Located along the linear route of Blackstock Street and Pumpfields Road, the Grand Arch will facilitate connections to the Ten Streets area and the waterfront beyond.
- » A new public square will be created at the lower level, linked to the north–south linear public space running alongside the viaduct.
- » The SPD proposes the biodiversity of the green space above the Grand Arch, while also exploring opportunities for pedestrian access.

Arches Yard and railway viaduct

- » Arches Yard, located at the convergence of the disused viaduct and the operational railway line, will be activated through new employment and commercial uses.
- » Together with public realm improvements at the intersection of Whitley Street and Love Lane, and enhancements to the existing pedestrianised space at the end of Burlington Street, Arches Yard will serve as a key hinge point between the SPD area and the Ten Streets.
- » The existing biodiversity corridor above the disused railway arches will be strengthened and enhanced, with the potential for integrated pedestrian access.

Spatial guidance: Public space and nature

- Development proposals within the Railway Edge Character Area must contribute to the enhancement of the existing biodiversity corridor along the disused railway arches and the Grand Arch.
- Proposals must also support the creation of a new public square around the Grand Arch, with potential future pedestrian and cycling links to the Ten Streets SPD area.
- Development proposals must allow for a north–south linear public space connecting the Grand Arch with the Arches Yard on Whitley Street.

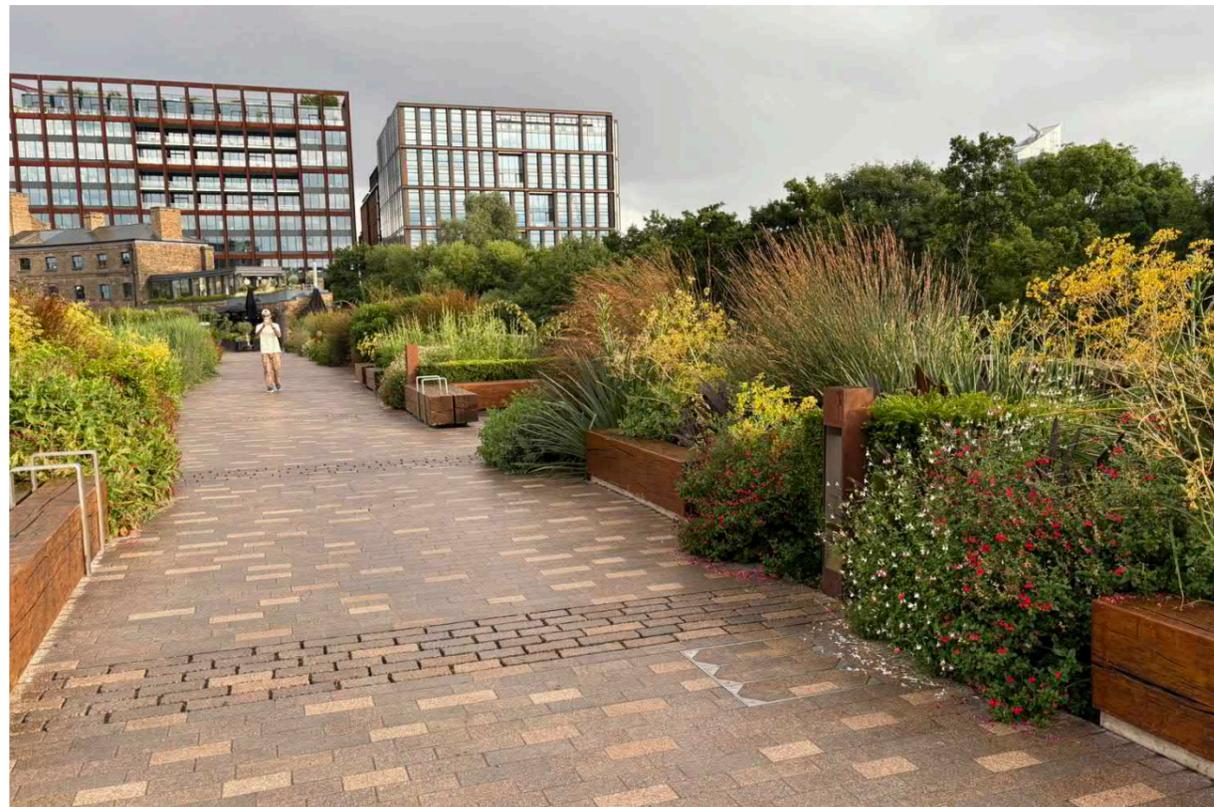


Fig 135 Public space and planting above railway viaduct



Fig 136 Linear public space along railway viaduct

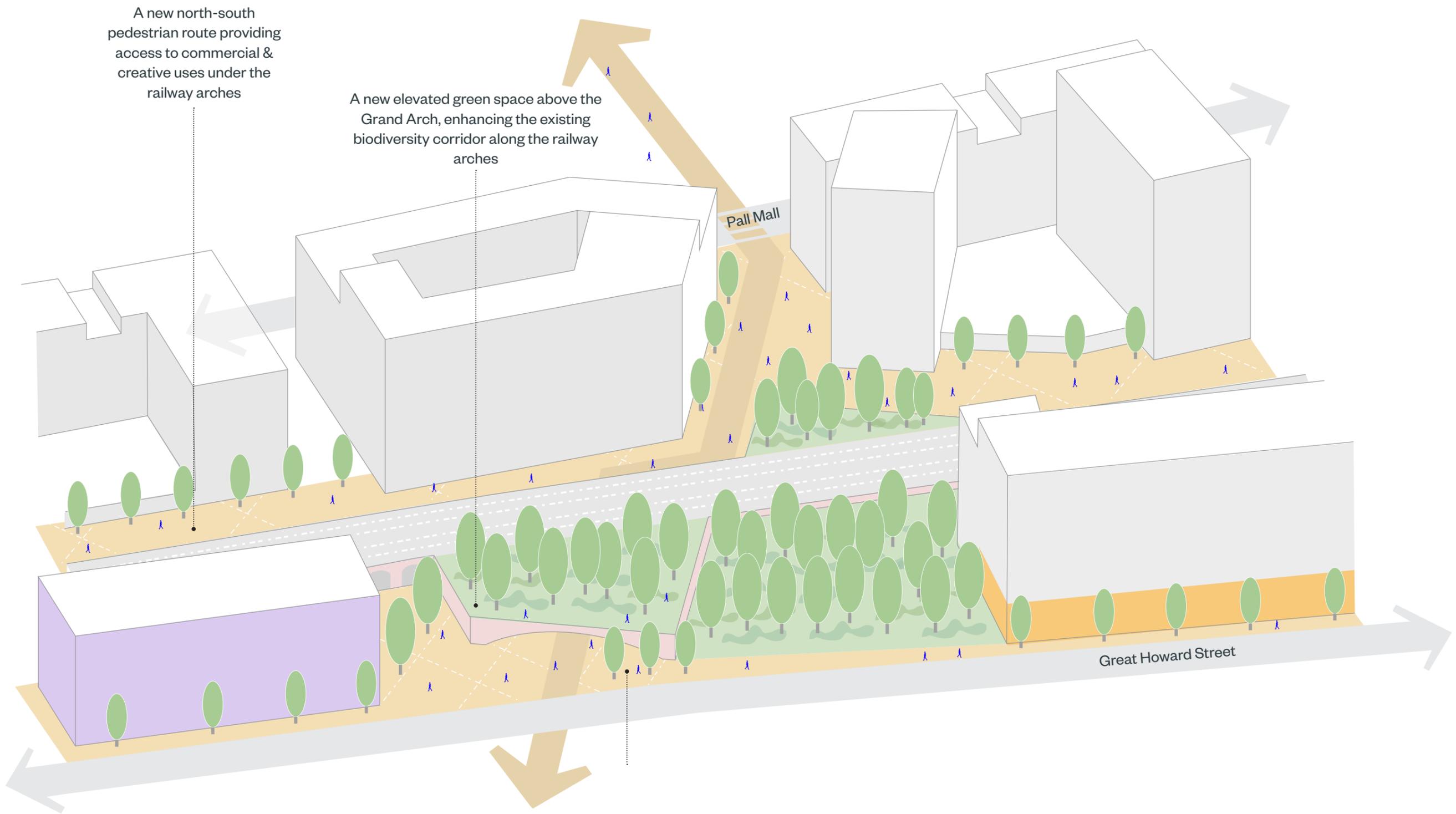


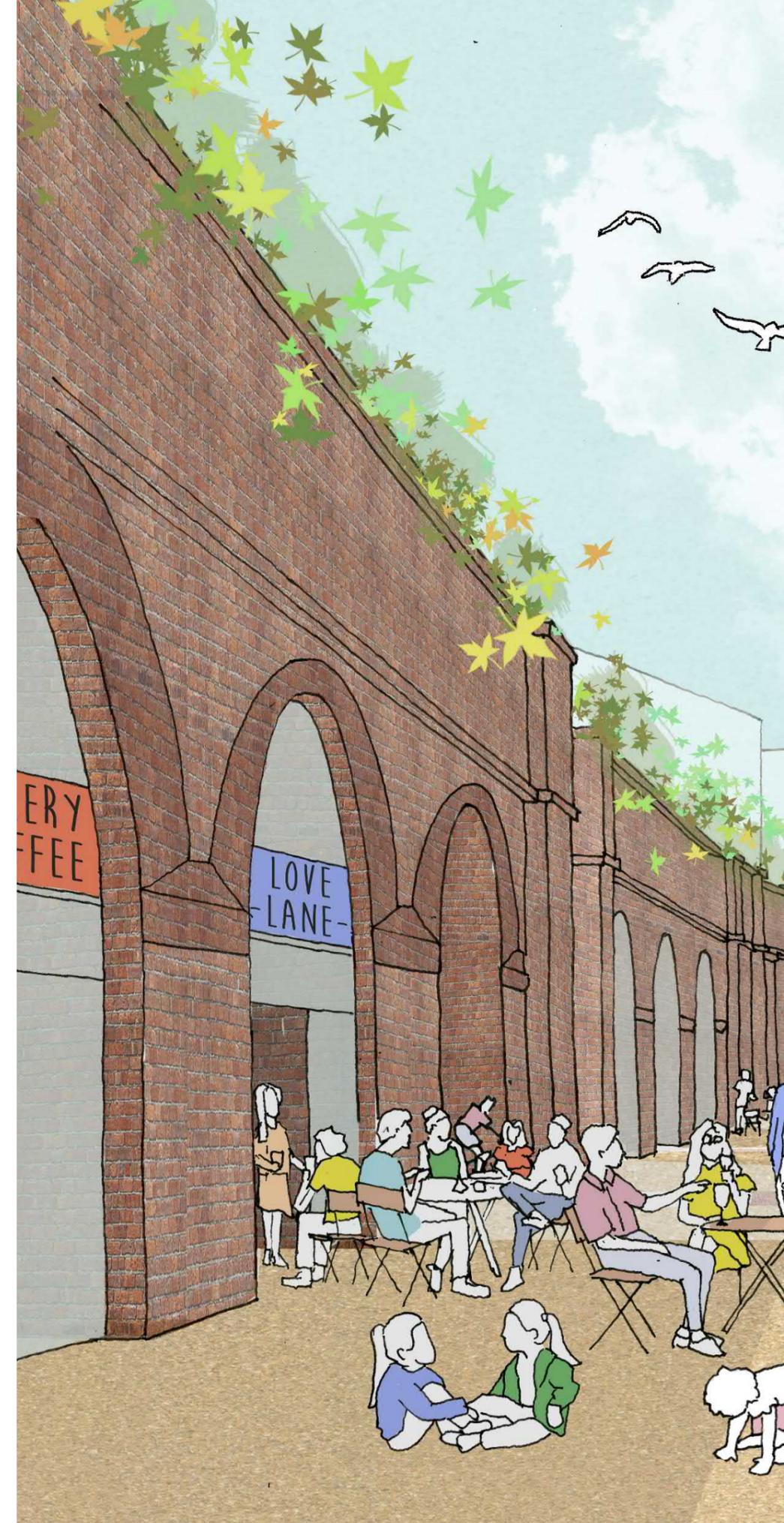
Fig 137 The Grand Arch

4.6.4 Built form

- » Built form and massing within the Railway Edge area will range from larger, taller apartment buildings at the southern end along Great Howard Street and Pall Mall, to low-rise buildings at the northern edge, along Love Lane.
- » Buildings along the eastern edge of Love Lane will help secure the rear garden boundaries of the Eldonian Village. These will comprise 3–4 storey buildings, with employment uses at ground and first floor levels, and apartments on the second and third floors.
- » This variation in scale will support a gradual transition from the City Centre Inner Core to the residential neighbourhoods of North Liverpool.
- » The railway viaduct will form the backbone of the area, with buildings positively engaging with the arches, which will be reactivated through non-residential uses.

Spatial guidance: Built form

- a. The built form should respond positively to the setting of the railway arches (non-designated heritage asset).
- b. Development proposals should explore the potential to incorporate design cues from the proportions, materials, and datum heights of the railway arches into the elevation design.
- c. Apartment buildings should maximise the number of dual-aspect dwellings, avoid single-aspect north-facing units, minimise internal corridor lengths, and ensure high standards of natural light and ventilation.
- d. Both residential and non-residential frontages should promote passive surveillance and contribute to a safe and welcoming public realm.
- e. Building heights within this area should range from ten storeys at the southern end, adjacent to the City Centre Fringe, to three to four storeys at the northern edge, where a lower scale is more appropriate due to the proximity of the Eldonian Village.
- f. Dwellings along Love Lane should consist of upper floor apartments and must be designed to minimise overlooking and preserve the privacy of existing residents in the Eldonian Village.
- g. Balconies along Pall Mall and Great Howard Street must be inset.
- h. Roofscape design should take cues from the area's character and industrial heritage. Pitched roofs, sawtooth roofs, and flat roofs are considered acceptable, while mansard roofs must be avoided.
- i. The Mobility Hub should be designed to high architectural standards, with flexibility in mind, ensuring the structure can adapt to future mobility patterns and evolving community needs.





Ten Streets SPD
parameter volumes

The Railway Edge will become a dynamic connector and catalyst for regeneration, transforming the railway arches into spaces for businesses, makers and creatives, while linking to Ten Streets and unlocking east-west movement.



Fig 138 Pitched roofs to residential buildings taking cues from the industrial history of the place



Fig 139 Maisonettes and townhouses securing an existing boundary through a back-to-back approach



Fig 140 Building steps down to respond to existing surrounding context

4.6.5 Uses

- » Employment and commercial uses within this area will be strengthened.
- » The railway arches and ground floor frontages along Love Lane will be activated with employment and commercial uses, including workshops and studios for creatives and makers, strengthening the SPD's ties to the Ten Streets creative hub area.
- » Ground floor commercial uses will also be introduced along Great Howard Street, reactivating frontages along this major route into the city centre.
- » Upper-floor dwellings with ground-floor communal entrances will complement the non-residential uses, providing overlooking and passive surveillance after business hours.
- » The Mobility Hub will be designed as a flexible and sustainable facility, offering efficient and functional parking solutions alongside a range of mobility services. It will explore how a long-term, circular parking structure can be developed to meet future, and partly unknown, mobility needs, with the potential for reuse, disassembly, conversion to alternative functions, or adaptation to new forms of mobility.

Spatial guidance: Uses

- a. Development proposals should incorporate employment and commercial uses at ground floor level along Love Lane and Great Howard Street.
- b. Upper floors should accommodate dwellings, in the form of upper floor apartments.
- c. Proposals should also positively engage with the railway arches, contributing to their reactivation.
- d. The Mobility Hub must be designed for potential reuse, disassembly, conversion to alternative functions, or adaptation to new forms of mobility.



Fig 141 Non-residential uses re-activating heritage railway arches

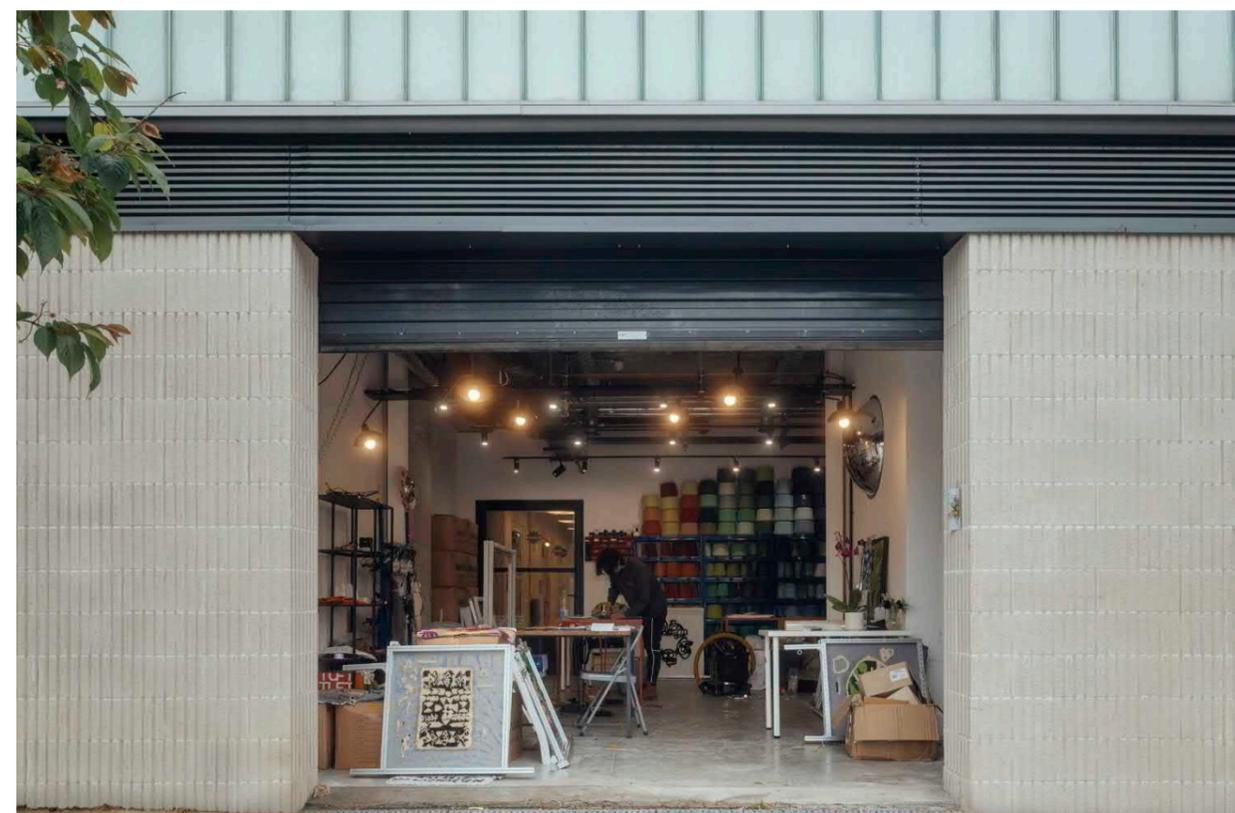


Fig 142 Workshops for makers and creatives activating the ground floor

4.7 Oriel Street & Cherry Lane

4.7.1 Vision & identity

- » The Oriel Street & Cherry Lane Character Area is defined by Vauxhall Road to the west, Gardners Row to the east, Paul Street to the north, and Naylor Street to the south.
- » The area is largely occupied by the Unite Student Accommodation and other pipeline developments that already have planning approval.
- » This SPD sets out both a short-term and long-term vision for the area.
- » Short-term, improvements will focus on enhancing the public realm and streetscape, including the introduction of better-quality pavements, improved street lighting, and tree planting. Infill development will primarily take place along the western edge of the area.
- » Long-term, as the area regenerates, there may be opportunities for the redevelopment of the Unite site.
- » Re-development proposals should aim to reintroduce east-west and north-south permeability, which is currently blocked by the perimeter wall surrounding the site.

KEY			
	SPD boundary		Unite Student Accomm.
	Heritage assets		8 Oriel Street
	SPD proposed buildings		9 Naylor Street
	Long term development opportunities		Foundry Yard
	Non-residential frontage		
	Inset balconies		
	Landmark building		
	SuDS		

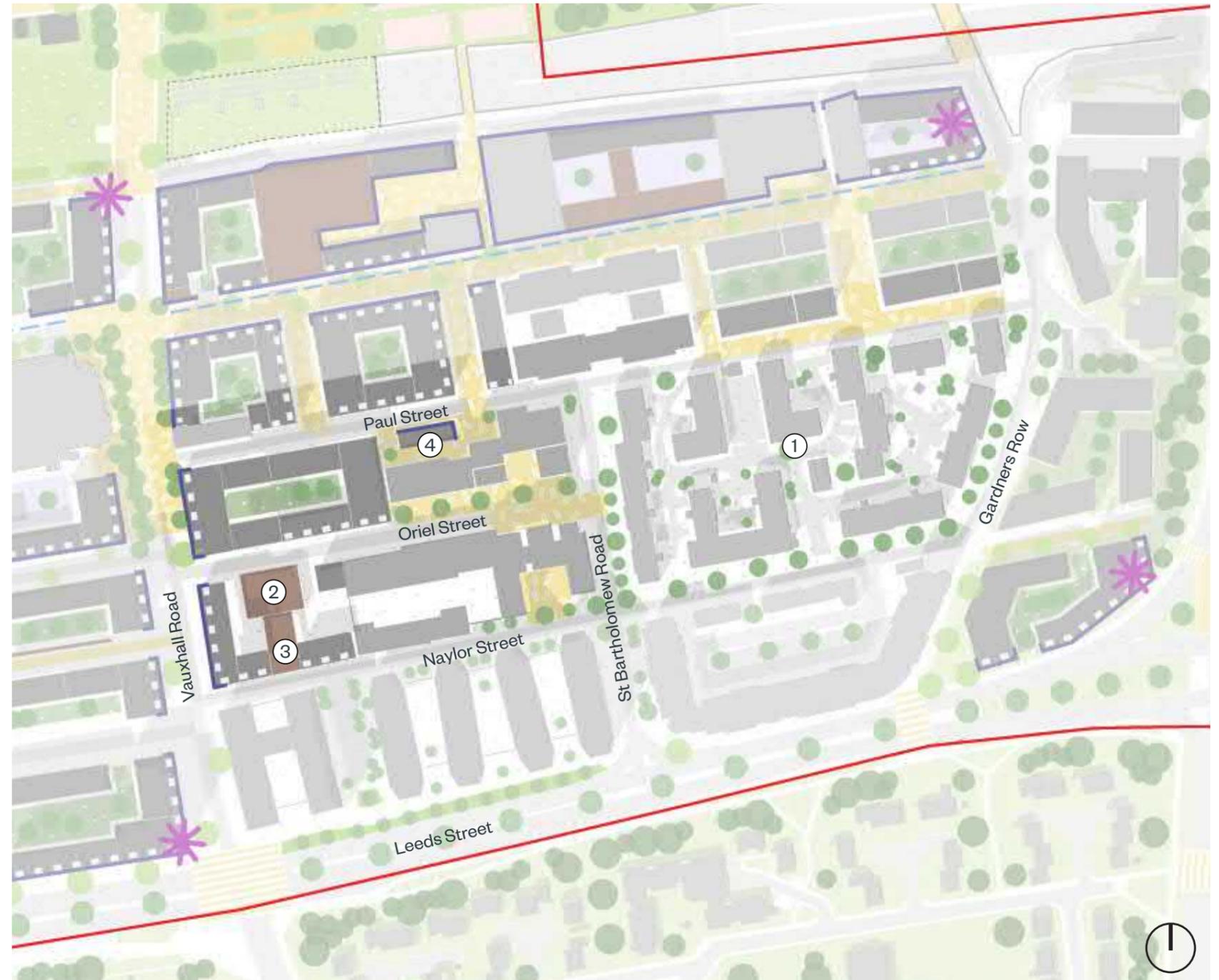


Fig 143 Oriel Street & Cherry Lane Character Area

- » Permeability will also be improved through the reinstatement of Cherry Lane, which once run through the area, as a pedestrian priority route, linking St Bartholomew Road with Gardners Row.
- » Cherry Lane will terminate at a new neighbourhood park, Cherry Gardens, which will act as a stepping stone in the green infrastructure network, connecting Ennerdale Park to the north of the Kingsway Tunnel with Marybone Green to the south of Leeds Street.

Spatial guidance: Vision & identity

- a. Development proposals within the Oriel Street & Cherry Lane Character Area must demonstrate how they contribute to the vision for the area set out in this SPD.

KEY

-  SPD boundary
-  Heritage assets
-  SPD proposed buildings
-  Long term development opportunities
-  Non-residential frontage
-  Inset balconies
-  Landmark building
-  SuDS
-  Cherry Gardens



Fig 144 Oriel Street & Cherry Lane Character Area (long-term vision)

4.7.2 Movement

Gardners Row

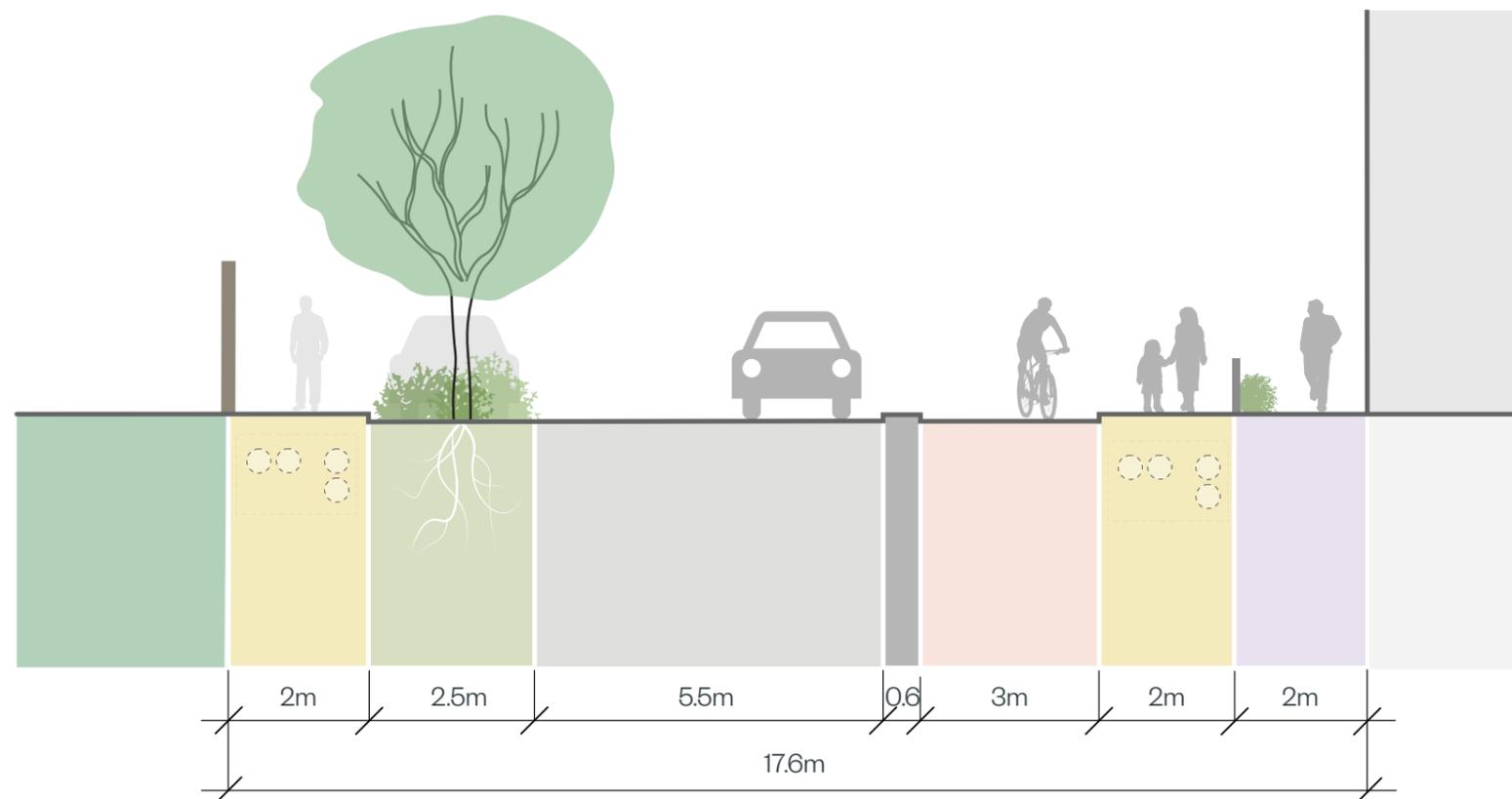
- » Gardners Row will be transformed into a key north-south active travel route, linking to the Limekiln Bridge and the residential neighbourhoods north of the Kingsway Tunnel.
- » A new segregated cycle route will be introduced alongside upgraded pavements and tree planting..
- » On-street parallel parking will be integrated alongside the trees to ensure parked cars do not dominate the streetscape.

Spatial guidance: Movement

- a. Development proposals within the Oriel Street & Cherry Lane Character Area must demonstrate how they respond to and contribute to the design principles for Gardners Row, set out in this SPD



Fig 145 Gardners Row today



- KEY
- Carriageway
 - Footpath with underground service corridor
 - Cycle Lane
 - Tree/planting zone with on street parking
 - Building zone (front garden)

Fig 146 Gardners Row proposed section

4.7.3 Public space and nature

Foundry Yard

- » Foundry Yard will be a small-scale public space linked to Blackstock Street via a pedestrian-only route.
- » Located adjacent to a proposed public realm intervention (approved under a separate planning application), Foundry Yard will serve as a stepping stone toward a potential new pedestrian priority route through the Unite Student Accommodation site. In the long term, this route will terminate at Cherry Gardens.
- » The yard will be activated by a small pavilion building accommodating community/culture uses.

Cherry Gardens

- » Cherry Gardens is a long-term aspiration for a new neighbourhood green space opening onto Gardners Row, aimed at delivering much-needed green infrastructure at the eastern edge of the SPD area.
- » The green space will mark the termination of a new pedestrian priority route that follows the alignment of the historic Cherry Lane and will act as a key link in a green corridor, bridging Ennerdale Park to the north and Marybone Green to the south of Leeds Street.
- » It will feature tree planting, informal play space, and landscape elements that support biodiversity.

Spatial guidance: Public space and nature

- a. Development proposals within the Oriel Street & Cherry Lane Character Area should contribute to the reinstatement of the east-west historic route of Cherry Lane.
- b. Proposals should also support the creation of a new public space linking pedestrian movement to Blackstock Street (i.e. Foundry Yard).
- c. Development proposals must allow for a new green space opening onto Gardners Row (i.e. Cherry Gardens).

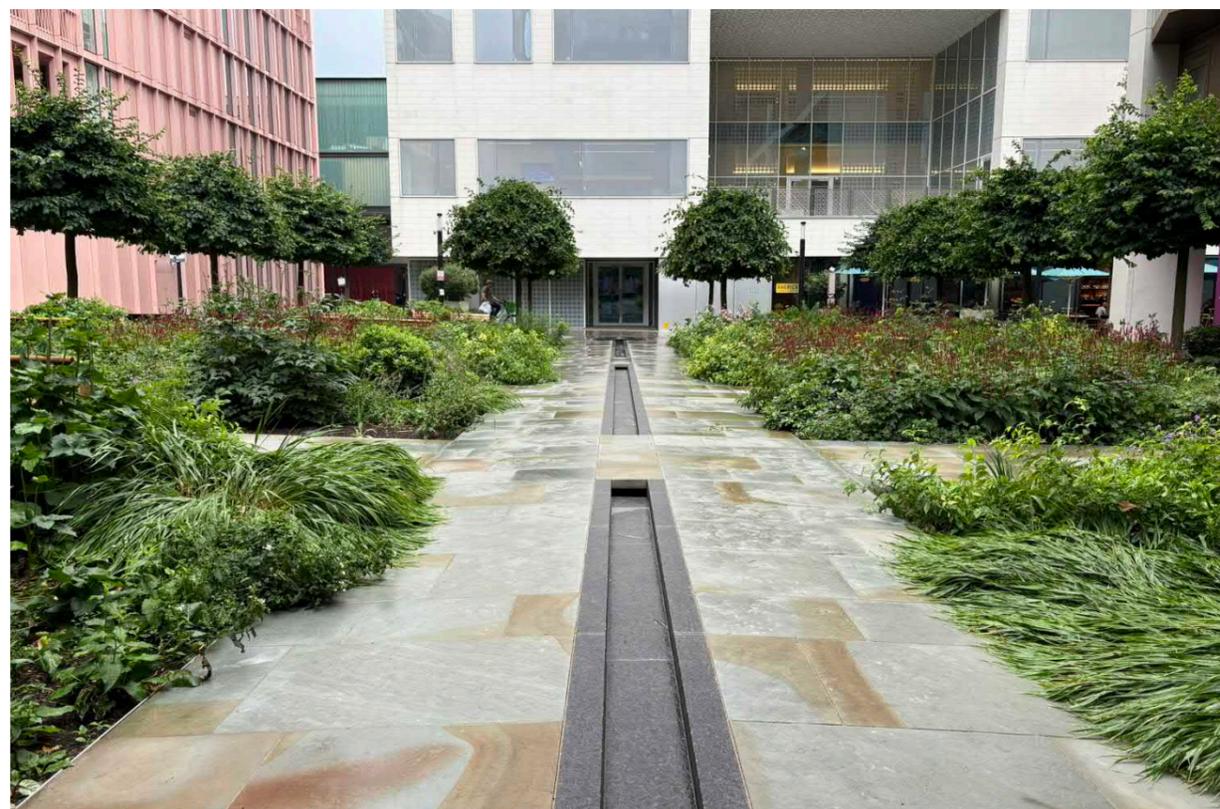


Fig 147 Public realm design celebrating the site's history and reinstating lost routes to enhance connectivity and sense of place.

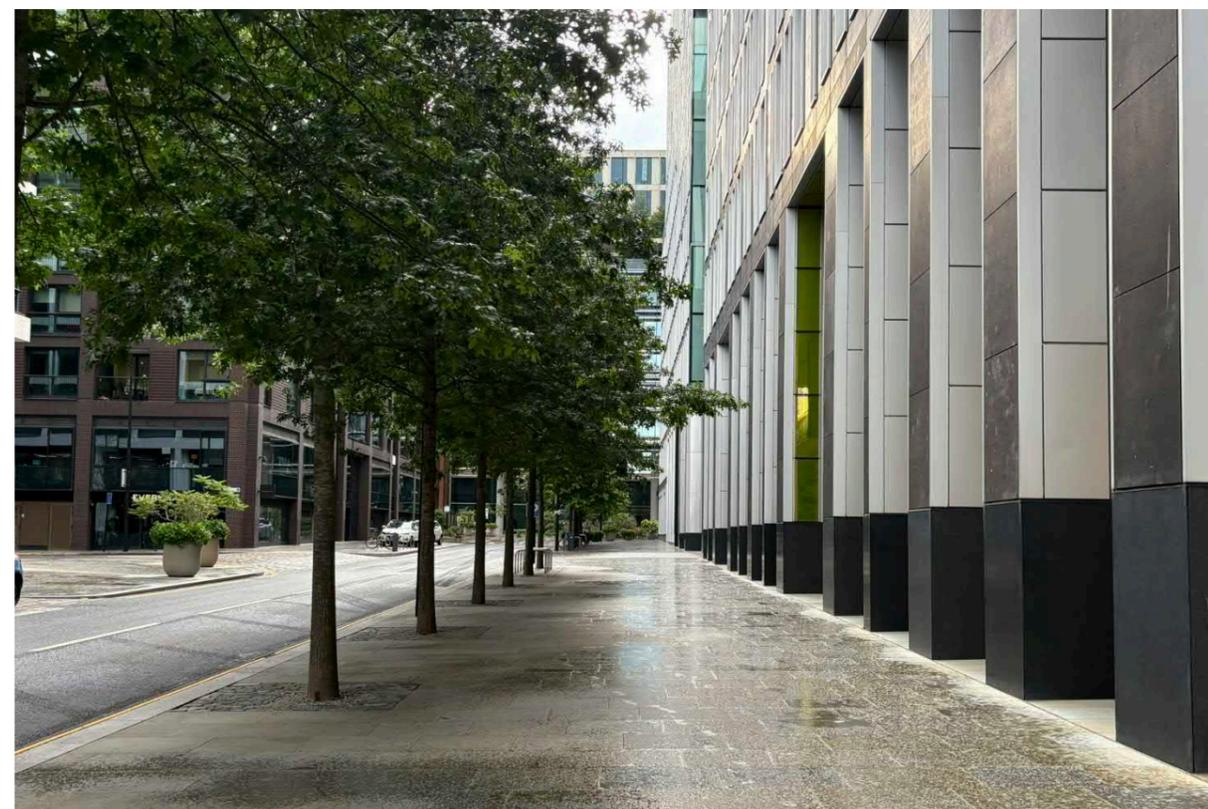


Fig 148 Generous pavement with tree planting

4.7.4 Built form

- » In the short term, built form and massing within the Oriel Street & Cherry Lane area include infill apartment buildings at the western end along Vauxhall Road.
- » In the longer term, the area represent a great opportunity to maximise the provision of new homes of all size and tenures through the redevelopment of the low-rise Unite Student Accommodation which is located within the Leeds Street Tall Buildings Cluster.
- » Paul Street, Oriel Street and Naylor Street will be activated by ground floor maisonettes with front doors opening directly onto the street, providing passive surveillance and reflecting the more intimate, human scale of the surrounding streetscape.

Spatial guidance: Built form

- a. The built form should respond positively to the setting of the non-designated heritage assets (i.e. 8 Oriel Street and 9 Naylor Street).
- b. Apartment buildings should maximise the number of dual-aspect dwellings, avoid single-aspect north-facing homes, minimise internal corridor lengths, and ensure high standards of natural light and ventilation.
- c. Both residential and non-residential frontages should promote passive surveillance and contribute to a safe and welcoming public realm.
- d. Building heights within this area should range from five to maximum ten storeys between Paul Street and Oriel Street, with taller buildings along Naylor Street and Gardners Row, where the area falls within the CL3a Tall Buildings Cluster (max. 45m / 15 residential storeys).
- e. Dwellings along Paul Street, Oriel Street and Naylor Street should consider the inclusion of maisonettes to reflect the smaller scale of the streets.
- f. Balconies along Vauxhall Road must be inset.
- g. Roofscape design should take cues from the area's character and industrial heritage. Pitched roofs, sawtooth roofs, and flat roofs are considered acceptable, while mansard roofs must be avoided.



Fig 149 Varying building heights contribute visual interest and rhythm to the built form

4.7.5 Uses

- » The Oriel Street and Cherry Lane area will be predominantly residential, with ground floor commercial uses focused along Vauxhall Road, complemented by other smaller-scale convenience uses within the area to support both new and existing residents.
- » A small pavilion building at Foundry Yard will accommodate community/culture uses and serve as an anchor along the pedestrian route from Blackstock Street to Gardners Row.
- » The area is currently largely occupied by Unite Student Accommodation, which may come forward for redevelopment in the longer term. If consistent with future needs at the time of redevelopment, the site could continue to provide student housing due to its proximity to Liverpool John Moores University.

Spatial guidance: Uses

- a. Development proposals should provide new residential uses and include commercial uses at ground floor level along Vauxhall Road, with smaller commercial units throughout the rest of the area to support both new and existing residents.



Fig 150 An intimate, publicly accessible yard enhanced with tree planting and integrated seating to create a welcoming place for rest and interaction

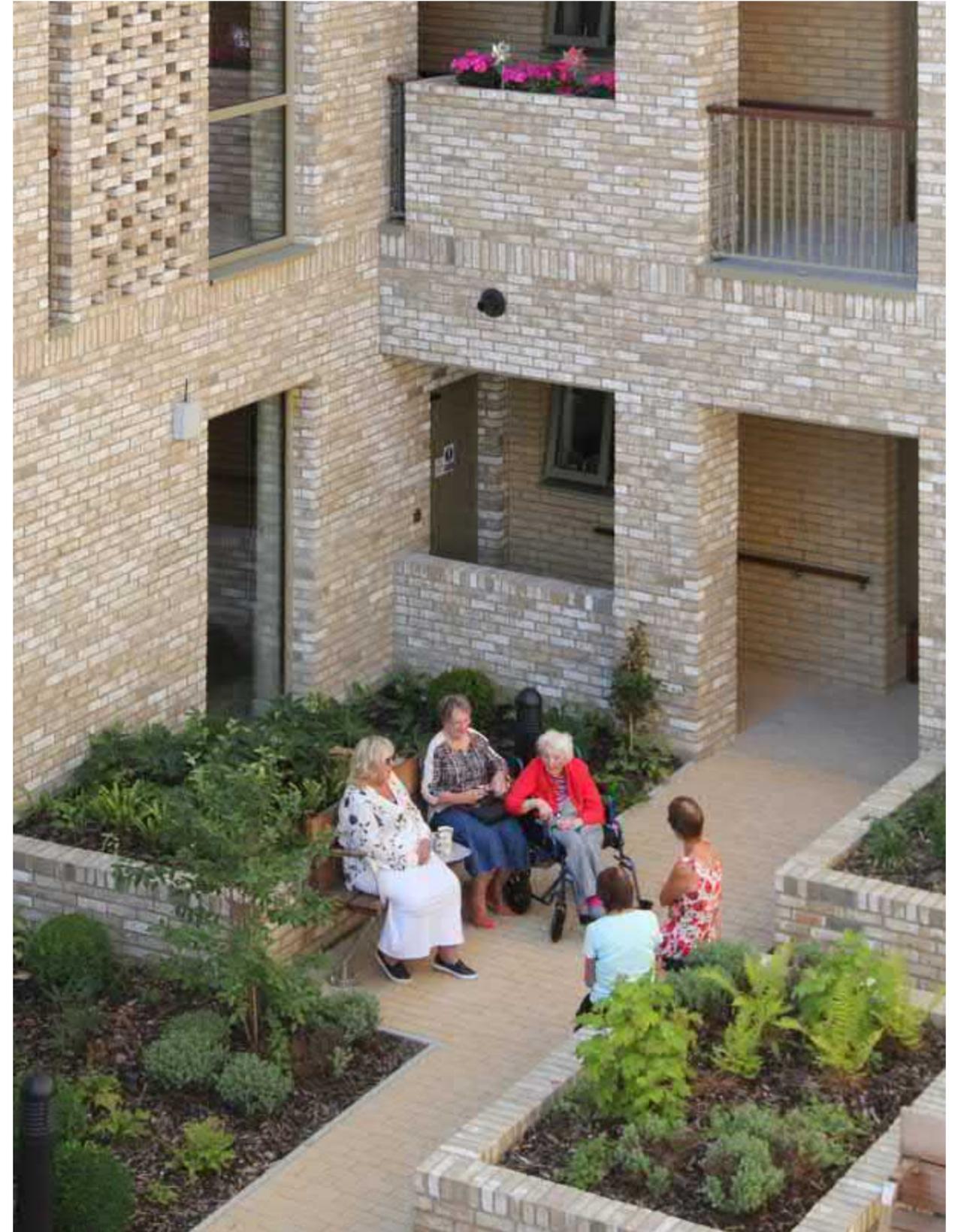


Fig 151 Residential amenity courtyards provide a safe space for rest and relaxation